

FHWA-Indiana Environmental Document
CATEGORICAL EXCLUSION / ENVIRONMENTAL ASSESSMENT FORM
GENERAL PROJECT INFORMATION

Road No./County:	County Road 250 West (CR 250 W) / Porter County
Designation Number(s):	2003041
Project Description/Termini:	Bridge Project on CR 250 W over Phillips Ditch / 1.51 to 1.64 miles south of State Road 8 (SR 8)

X	Categorical Exclusion, Level 2 – Required Signatories: INDOT DE and/or INDOT ESD
	Categorical Exclusion, Level 3 – Required Signatories: INDOT ESD
	Categorical Exclusion, Level 4 – Required Signatories: INDOT ESD and FHWA
	Environmental Assessment (EA) – Required Signatories: INDOT ESD and FHWA
	Additional Investigation (AI) – The proposed action included a design change from the original approved environmental document. Required Signatories must include the appropriate environmental approval authority

Approval _____
INDOT DE Signature and Date
INDOT ESD Signature and Date

 FHWA Signature and Date

Release for Public Involvement _____
N/A
ADWP
February 7, 2024
INDOT DE Initials and Date
INDOT ESD Initials and Date

Certification of Public Involvement _____
 INDOT Consultant Services Signature and Date

INDOT DE/ESD Reviewer Signature and Date: _____

Name and Organization of CE/EA Preparer: Carson Hoogewerf/ Lochmueller Group, Inc.

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Note: Refer to the most current INDOT CE Manual, guidance language, and other ESD resources for further guidance regarding any section of this form.

Part I – Public Involvement

Every Federal action requires some level of public involvement, providing for early and continuous opportunities throughout the project development process. **The level of public involvement should be commensurate with the proposed action.**

Does the project have a historic bridge processed under the Historic Bridges PA*?	Yes	No
	<input type="checkbox"/>	<input checked="" type="checkbox"/>
If No, then:		
Opportunity for a Public Hearing Required?	<input checked="" type="checkbox"/>	<input type="checkbox"/>

*A public hearing is required for all historic bridges processed under the Historic Bridges Programmatic Agreement between INDOT, FHWA, SHPO, and the ACHP.

Discuss what public involvement activities (legal notices, letters to affected property owners and residents (i.e. notice of entry), meetings, special purpose meetings, newspaper articles, etc.) have occurred for this project.

Notice of Entry letters were mailed to potentially affected property owners near the project area on February 25, 2022 notifying them about the project and that individuals responsible for land surveying and field activities may be seen in the area. A sample copy of the Notice of Entry letter is included in Appendix G, G1.

The project will meet the minimum requirements described in the current Indiana Department of Transportation (INDOT) Project Development Public Involvement Procedures Manual which requires the project sponsor to offer the public an opportunity to submit comments and/or request a public hearing. Therefore, a legal notice will appear in a local publication contingent upon the release of this document for public involvement. This document will be revised after the public involvement requirements are fulfilled.

Public Controversy on Environmental Grounds

Discuss public controversy concerning community and/or natural resource impacts, including what is being done during the project to minimize impacts.

At this time, there is no substantial public controversy concerning impacts to the community or to natural resources.

Part II - General Project Identification, Description, and Design Information

Sponsor of the Project: Porter County INDOT District: LaPorte

Local Name of the Facility: Porter County Bridge No. 66, located along CR 250 W (Bride Asset No. 64-00066)

Funding Source (mark all that apply): Federal State Local Other*

*If other is selected, please identify the funding source: _____

PURPOSE AND NEED:

The need should describe the specific transportation problem or deficiency that the project will address. The purpose should describe the goal or objective of the project. The solution to the traffic problem should NOT be discussed in this section.

Need: The need for the project stems from the deteriorated condition of the existing bridge. According to the Bridge Inspection Report dated May 23, 2022 (Appendix I, I2 to I10), the condition ratings of the bridge deck, superstructure, and substructure were 6 (satisfactory condition), 6 (satisfactory condition), and 5 (fair condition) out of 9 (excellent condition), respectively. The overall condition rating of the bridge is 5 (fair condition) out of 9. Condition ratings range from 0, which represents a failed structure, to 9, which represents a new structure with no deficiencies. The bridge deck exhibited moderate seepage with efflorescence between beams. The superstructure exhibited moderate efflorescence at beam joints and widening with newer beams. The substructure exhibited widening at the east abutment ends, deterioration at the north abutment including cracks and an offset footing, and

This is page 2 of 23 Project name: Replacement of Porter County Bridge No. 66 Date: February 6, 2024

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spalling. The bridge is identified as scour critical in the May 23, 2022 Bridge Inspection Report and was placed on a 24 month inspection schedule.

Purpose: The purpose for the project is to improve the overall condition of the structure and its components to a condition rating of at least 7 ("good") out of 9.

PROJECT DESCRIPTION (PREFERRED ALTERNATIVE):

County: Porter

Municipality: N/A

Limits of Proposed Work: CR 250 W, from 1.51 to 1.64 miles south of SR 8

Total Work Length: 0.13 Mile(s)

Total Work Area: 0.86 Acre(s)

Is an Interstate Access Document (IAD)¹ required?
If yes, when did the FHWA provide a Determination of Engineering and Operational Acceptability?

Yes ¹	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>
Date: <input style="width: 100%;" type="text"/>	

¹If an IAD is required; a copy of the approved CE/EA document must be submitted to the FHWA with a request for final approval of the IAD.

Describe location of project including township, range, city, county, roads, etc. Existing conditions should include current conditions, current deficiencies, roadway description, surrounding features, etc. Preferred alternative should include the scope of work, anticipated impacts, and how the project will meet the Purpose and Need. Logical termini and independent utility also need discussed.

Porter County and the Federal Highway Administration (FHWA), with oversight by the Indiana Department of Transportation (INDOT) LaPorte District, intend to proceed with a bridge project along CR 250 W in Porter County (Des. No. 2003041). The project involves Bridge No. 66 (Asset No. 64-00066, National Bridge Inventory (NBI) No. 6400044), which carries CR 250 W over Phillips Ditch.

Location:

The proposed project is located in Porter County along CR 250 W approximately 1.51 to 1.64 miles south of SR 8. Specifically, the project is located in Section 21, Township 33 North, Range 6 West in Boone Township as depicted on the Kouts U.S. Geological Survey 1:24,000 scale quadrangle map. Adjacent land use is rural and consists of agriculture, streams, riparian areas, and residential areas.

Existing Conditions:

CR 250 W is functionally classified as a rural major collector. The typical cross section of CR 250 W consists of two 10-foot wide travel lanes, one in each direction, with 2-foot-wide aggregate outside shoulders. The posted speed limit at the project location is 45 miles per hour. Bridge No. 66 was built in 1930 and reconstructed in 1996 and is a single-span prestressed concrete adjacent box beam bridge. The clear roadway width of the bridge is 30.0 feet, including two 10-foot-wide lanes and two 5-foot-wide outside shoulders. The out-to-out deck width is 30.6 feet, and the bridge length is 52.5 feet. Drainage is conveyed along the northwest side of CR 250 W via an unnamed tributary (UNT) to Keller Ditch, which flows south into the project area and turns west. Within the construction limits, drainage is conveyed within shallow roadside depressions which flow to Phillips Ditch. Phillips Ditch flows east to west underneath CR 250 W via Bridge No. 66. Existing deficiencies of the bridge include wear and cracking of the wearing surface, beam widening affecting the bridge deck, superstructure, and substructure, beam efflorescence, and deterioration, cracking, and spalling of the substructure. The bridge is scour critical (Appendix I, I2 to I10). Adjacent land use is rural and consists of active agriculture and residential areas (Appendix B, B3).

Preferred Alternative:

The preferred alternative involves the removal of Bridge No. 66 and construction of a new replacement bridge over Phillips Ditch. The replacement structure is expected to be a single-span prestressed concrete box beam bridge featuring two 11-foot wide travel lanes with 4-foot shoulders and standard concrete railing atop the bridge deck. The clear roadway width and out-to-out width will be 30 feet and 33 feet, respectively, and the bridge will be approximately 63.75 feet in length.

Roadway approach work is expected to include full-depth pavement replacement and milling and hot mix asphalt (HMA) resurfacing. Pavement replacement will extend approximately 277 feet south and 214 feet north of the bridge, with 119 feet of paving exception in between for bridge construction. The roadway section will include two 11-foot lanes with 2- to 4-foot paved (3- to 5-foot usable)

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shoulders. Incidental construction will extend 50 feet on either end of the construction limits and will include 25 feet of pavement reconstruction and 25 feet of 1.5-inch milling and HMA overlay on both ends. Riprap drainage turnouts will be constructed in all four bridge quadrants. Riprap will also be installed along the banks of Phillips Ditch. The total acreage of riprap is approximately 0.183 acre. Existing guardrail along CR 250 W totaling 171 linear feet will be removed, and 248 linear feet of new guardrail will be constructed along both approaches to the bridge. Including incidental construction, the total estimated length of the project is 710 feet (0.13 mile).

The maintenance of traffic (MOT) plan will include a full closure with detour route. See the *Maintenance of Traffic (MOT)* section for more information.

This project is anticipated to require 0.87 acre of new permanent right-of-way (ROW) and 0.07 acre of temporary ROW. A total of 140 linear feet of temporary impacts to streams are expected. A total of approximately 0.44 acre of impacts to terrestrial habitat are expected, including 0.15 acre of tree clearing. Every effort to avoid and minimize impacts to environmental resources will be made to the greatest extent possible. Complete avoidance of these resources is not practicable to meet the purpose and need of the project. No mitigation is expected.

The project meets the purpose and need by addressing the deficiencies identified with the structure and improving the condition rating of the bridge components to a rating of 7 (good condition) or higher.

Logical Termini/Independent Utility:

The termini of the project occur along CR 250 W approximately 1.51 miles to 1.64 miles south of SR 8 (0.13 mile). The termini of the project provide the logical beginning and end point necessary to complete the bridge improvements. The project is independent of any other action and able to be constructed without relying on the completion of any other project.

OTHER ALTERNATIVES CONSIDERED:

Provide a header for each alternative. Describe all discarded alternatives, including the No Build Alternative. Explain why each discarded alternative was not selected. Make sure to state how each alternative meets or does not meet the Purpose and Need and why.

Bridge Rehabilitation: This alternative would involve rehabilitation of the superstructure and substructure of the existing bridge. While this alternative would have met the purpose and need of the project and may have reduced the environmental impacts of the project, it was determined that rehabilitation of the bridge would not be practicable due to the extent of deterioration of the substructure. Substructure rehabilitation would necessitate dismantling of the bridge deck, which necessitates replacement. Therefore, this alternative was not given further consideration.

No Build Alternative: This alternative would involve no improvements to the existing bridge. While this alternative would alleviate environmental impacts, deterioration of the bridge would continue and the purpose and need of the project would not be met. Therefore, this alternative was not given further consideration.

The No Build Alternative is not feasible, prudent or practicable because (Mark all that apply)

- It would not correct existing capacity deficiencies;
- It would not correct existing safety hazards;
- It would not correct the existing roadway geometric deficiencies;
- It would not correct existing deteriorated conditions and maintenance problems; or
- It would result in serious impacts to the motoring public and general welfare of the economy.
- Other (Describe):

X

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ROADWAY CHARACTER:

If the proposed action includes multiple roadways, complete and duplicate for each roadway.

Name of Roadway:	<u>CR 250 W</u>			
Functional Classification:	<u>Rural Major Collector</u>			
Current ADT:	<u>868</u>	<u>VPD (2026)</u>	Design Year ADT:	<u>1059</u> <u>VPD (2046)</u>
Design Hour Volume (DHV):	<u>111</u>	Truck Percentage (%)	<u>18.1</u>	
Designed Speed (mph):	<u>45</u>	Legal Speed (mph):	<u>45</u>	

	Existing		Proposed
Number of Lanes:	2		2
Type of Lanes:	Through Travel		Through Travel
Pavement Width:	20	ft.	30
Shoulder Width:	4 (total)	ft.	8 (total)
Median Width:	N/A	ft.	N/A
Sidewalk Width:	N/A	ft.	N/A

Setting:	<input type="checkbox"/> Urban	<input type="checkbox"/> Suburban	<input checked="" type="checkbox"/> Rural
Topography:	<input checked="" type="checkbox"/> Level	<input type="checkbox"/> Rolling	<input type="checkbox"/> Hilly

BRIDGES AND/OR SMALL STRUCTURE(S):

If the proposed action includes multiple structures, complete and duplicate for each bridge and/or small structure. Include both existing and proposed bridge(s) and/or small structure(s) in this section.

Structure/NBI Number(s):	<u>Str. No. 64-00066/NBI No. 6400044</u>	Sufficiency Rating:	<u>70.4, Bridge Inspection Report 5/23/22 (Appendix I, I2 to I10)</u> (Rating, Source of Information)
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	Existing		Proposed
Bridge/Structure Type:	Prestressed concrete adjacent box beam bridge		Prestressed concrete adjacent box beam bridge
Number of Spans:	1		1
Weight Restrictions:	N/A	ton	N/A
Height Restrictions:	N/A	ft.	N/A
Curb to Curb Width:	30	ft.	30
Outside to Outside Width:	30.6	ft.	33
Shoulder Width:	10 (total)	ft.	8 (total)

Describe impacts and work involving bridge(s), culvert(s), pipe(s), and small structure(s). Provide details for small structure(s): structure number, type, size (length and dia.), location and impacts to water. Use a table if the number of small structures becomes large. If the table exceeds a complete page, put it in the appendix and summarize the information below with a citation to the table.

Porter County Bridge No. 66 was built in 1930 and reconstructed in 1996. It is not included in the Historic Bridge Inventory. The bridge is a single-span prestressed concrete adjacent box beam bridge. The clear roadway width is 30.0 feet, out-to-out deck width is 30.6 feet, and the bridge length is 52.5 feet. The preferred alternative involves the replacement of Bridge No. 66 with a new bridge over Phillips Ditch. The replacement structure is expected to be a single-span prestressed concrete box beam bridge featuring two 11-foot-wide travel lanes with 2 to 4-foot-wide paved (additional 1-foot-wide aggregate) shoulders and standard concrete railing atop the bridge deck. The clear roadway width and out to out width will be 30 feet and 33 feet, respectively, and the bridge will be approximately 63.75 feet in length. Riprap drainage turnouts will be constructed in all four bridge quadrants. Riprap will be placed along the banks of Phillips Ditch, but above the ordinary high water mark.

No other structure work is anticipated for this project.

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MAINTENANCE OF TRAFFIC (MOT) DURING CONSTRUCTION:

	Yes	No
Is a temporary bridge proposed?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Is a temporary roadway proposed?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Will the project involve the use of a detour or require a ramp closure? (describe below)	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Provisions will be made for access by local traffic and so posted.	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Provisions will be made for through-traffic dependent businesses.	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Provisions will be made to accommodate any local special events or festivals.	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Will the proposed MOT substantially change the environmental consequences of the action?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Is there substantial controversy associated with the proposed method for MOT?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Will the project require a sidewalk, curb ramp, and/or bicycle lane closure? (describe below)	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Provisions will be made for access by pedestrians and/or bicyclist and so posted (describe below).	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Discuss closures, detours, and/or facilities (if any) that will be provided for maintenance of traffic. Any known impacts from these temporary measures should be quantified to the extent possible, particularly with respect to properties such as Section 4(f) resources and wetlands. Discuss any pedestrian/bicycle closures. Any local concerns about access and traffic flow should be detailed as well.

The MOT for the project will require a full closure of CR 250 W with detour route. The detour route will utilize CR W 1700 N, CR 125 W (S Baums Bridge Road), and CR W 900 S (Appendix B, B16). The total distance of the detour is 8.6 miles, representing approximately 7 minutes of additional travel time. The MOT is expected to be in place for approximately four months. The MOT will be implemented per the *Manual on Uniform Traffic Control Devices* guidelines.

The Transportation Director for the Metropolitan School District of Boone Township responded to early coordination on January 10, 2023 indicating that a bus stop is located near the subject bridge, and that the associated detour would likely interfere with the existing bus route, likely requiring the bus to turn around and adding 10 to 15 minutes of travel time. The Transportation Director requested that, if possible, the detour be instated in the summertime when bus routes discontinue. The information was passed on to the project design team and to Porter County. Porter County indicated that limitation of the detour to the summer months to reduce impacts to the bus route was practicable and that coordination with the Area Engineer would occur during contract preparation to ensure a summer timeframe for the detour is provided. This is a firm project commitment.

The closures/lane restrictions will pose a temporary inconvenience to traveling motorists (including school buses and emergency services); however, with the above consideration to the detour schedule, no significant delays are anticipated, and all inconveniences and delays will cease upon project completion.

ESTIMATED PROJECT COST AND SCHEDULE:

Engineering: \$ 556,625 (2022, 2026) Right-of-Way: \$ 70,000 (2024) Construction: \$ 1,377,500 (2026)

Anticipated Start Date of Construction: Summer 2026

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RIGHT OF WAY:

Land Use Impacts	Amount (acres)	
	Permanent	Temporary
Residential	0.15	0.05
Commercial	0.0	0.0
Agricultural	0.66	0.02
Forest	0.0	0.0
Wetlands	0.0	0.0
Other: Stream	0.06	0.0
Other:	0.0	0.0
TOTAL	0.87	0.07

Describe both Permanent and Temporary right-of-way and describe their current use. Typical and Maximum right-of-way widths (existing and proposed) should also be discussed. Any advance acquisition, reacquisition or easements, either known or suspected, and their impacts on the environmental analysis should be discussed.

Existing ROW extends to the edge of pavement of CR 250 W, approximately 10 feet east and 10 feet west of the CR 250 W centerline, for a total typical and maximum width of 20 feet wide. Existing ROW consists of the existing roadway pavement.

The project will require approximately 0.87 acre of new permanent ROW, acquired from both sides of CR 250 W. New permanent ROW areas consist of agriculture, maintained roadside vegetation, residential areas, and Phillips Ditch. Activities occurring within new permanent ROW areas include roadway widening, drainage turnout construction, guardrail replacement, driveway work, placement of riprap, grading, and equipment access. The project will require approximately 0.07 acre of temporary ROW, located to the northeast, southeast, and southwest of the bridge. Temporary ROW areas consist of residential areas and agriculture. Activities occurring within temporary ROW areas include driveway work and equipment access. The proposed typical ROW width varies between 20 feet and 80 feet wide (10 feet and 40 feet, respectively, either side of the roadway centerline). The proposed maximum ROW width will reach approximately 135 feet in total width (70 feet west and 65 feet east of the roadway centerline).

If the scope of work or permanent or temporary right-of-way amounts change, the INDOT Environmental Services Division (ESD) and the INDOT LaPorte District Environmental Section will be contacted immediately. This is a firm commitment and is included in the *Environmental Commitments* section of this document.

Part III – Identification and Evaluation of Impacts of the Proposed Action

SECTION A - EARLY COORDINATION:

List the date(s) coordination was sent and all resource agencies that were contacted as a part of the development of this Environmental Study. Also, include the date of their response or indicate that no response was received.

Early coordination letters were sent on January 10, 2023 (Appendix C, C1 to C4).

Agency	Date Sent	Date Response Received	Appendix C
Federal Highway Administration – Indiana Division	January 10, 2023	No Response Received	N/A
U.S. Department of Agriculture, Natural Resources Conservation Service	January 10, 2023	January 19, 2023	C13 to C14
Chicago Regional Office, U.S. Department of Housing and Urban Development	January 10, 2023	No Response Received	N/A
National Park Service, Midwest	January 10, 2023	No Response Received	N/A

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Regional Office			
U.S. Army Corps of Engineers, Chicago District	January 10, 2023	January 12, 2023; January 19, 2023	C10 to C11
U.S. Coast Guard, Ninth District	January 10, 2023	January 17, 2023	C12
INDOT LaPorte District Office, Environmental Section Manager	January 10, 2023	January 12, 2023 (NO COMMENT)	C9
INDOT LaPorte District Office, Project Manager	January 10, 2023	No Response Received	N/A
INDOT Environmental Services Division	January 10, 2023	No Response Received	N/A
IDNR, Division of Fish and Wildlife	January 10, 2023	February 8, 2023	C15 to C17
Indiana Geological and Water Survey	January 10, 2023	January 10, 2023	C5 to C6
Northern Indiana Regional Planning Commission	January 10, 2023	No Response Received	N/A
Porter County Highway Department	January 10, 2023	No Response Received	N/A
Porter County Stormwater Board (Drainage Board)	January 10, 2023	No Response Received	N/A
Porter County Board of Commissioners	January 10, 2023	No Response Received	N/A
Porter County Council	January 10, 2023	No Response Received	N/A
Porter County Township Trustees, Boone Township	January 10, 2023	No Response Received	N/A
Porter County Surveyor's Office	January 10, 2023	No Response Received	N/A
Porter County Emergency Management Agency	January 10, 2023	No Response Received	N/A
Porter County E911	January 10, 2023	No Response Received	N/A
Porter County Sheriff Department	January 10, 2023	No Response Received	N/A
Porter County MS4 Coordinator	January 10, 2023	No Response Received	N/A
Porter County Floodplain Administrator	January 10, 2023	No Response Received	N/A
Northwest Health - Porter Ambulance Service	January 10, 2023	No Response Received	N/A
Kouts Police Department	January 10, 2023	No Response Received	N/A
Kouts Fire Department	January 10, 2023	No Response Received	N/A
Metropolitan School District of Boone Township	January 10, 2023	January 10, 2023; January 13, 2023	C7 to C8

All applicable recommendations are included in the *Environmental Commitments* section of this Categorical Exclusion (CE) document.

SECTION B – ECOLOGICAL RESOURCES:

Streams, Rivers, Watercourses & Other Jurisdictional Features

- Federal Wild and Scenic Rivers
- State Natural, Scenic or Recreational Rivers
- Nationwide Rivers Inventory (NRI) listed
- Outstanding Rivers List for Indiana
- Navigable Waterways

Presence

X

Impacts

Yes	No
X	

Total stream(s) in project area: 850 Linear feet Total impacted stream(s): 140 Linear feet

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Stream Name	Classification	Total Size in Project Area (linear feet)	Impacted linear feet	Comments (i.e. location, flow direction, likely Water of the US, appendix reference)
Phillips Ditch	Perennial	336	140	(41.29550°, -87.11270°), Flow NE to SW, Likely WOTUS, Appendix F
UNT 1 to Keller Ditch	Intermittent	514	0	(41.29700°, -87.11280°) Flow N to S to W, Likely WOTUS, Appendix F

Describe all streams, rivers, watercourses and other jurisdictional features adjacent or within the project area. Include whether or not impacts (both permanent and temporary) will occur to the features identified. Include if the streams or rivers are listed on any federal or state lists for Indiana. Include if features are likely subject to federal or state jurisdiction. Discuss measures to avoid, minimize, and mitigate if impacts will occur.

Based on the desktop review, the aerial map of the project area (Appendix B, B3), and the Red Flag Investigation (RFI) report (Appendix E, E1 to E6) there are 13 streams, rivers, watercourses, or other jurisdictional features within the 0.5-mile search radius. There is one stream located within or adjacent to the project area. That number was updated to two by the site visit on August 2, 2022 by Lochmueller Group.

There are no Federal Wild and Scenic Rivers; State Natural, Scenic, and Recreational Rivers; Outstanding Rivers for Indiana; navigable waterways; or National Rivers Inventory waterways present in the project area.

Phillips Ditch is listed as impaired for *E. coli*. Workers who are working in or near water with *E. coli* should take care to wear appropriate PPE, observe proper hygiene procedures, including regular hand washing, and limit personal exposure. This is a firm commitment and is included in the *Environmental Commitments* section of this document.

Phillips Ditch is a Porter County regulated drain. No regulated drain permit is required in Porter County.

A *Waters of the U.S. Determination Report* was completed for the project on September 19, 2022. Please refer to Appendix F, F1 to F17 for the *Waters of the U.S. Determination Report*. It was determined that two streams, Phillips Ditch and UNT 1 to Keller Ditch, are present within the project area. The U.S. Army Corps of Engineers (USACE) makes all final determinations regarding jurisdiction.

Phillips Ditch

Phillips Ditch is a perennial stream that flows from northeast to southwest through the investigation area. This reach of Phillips Ditch has been excavated and channelized to convey agricultural drainage. The stream has a moderate flow. The banks are steep but well vegetated and have minimal signs of scour and undercutting. Approximately 336 feet of the stream is within the investigated project area, and approximately 143 feet of the stream is within the limits of construction. The Ordinary High Water Mark (OHWM) is 22.5 feet wide by 0.67 feet deep. The upstream drainage area is 19.053 square miles. The substrate within this reach of Phillips Ditch is silt (60%) and sand (40%). The channel morphology within this reach of Phillips Ditch is predominately run (90%) with some pools present (10%).

Phillips Ditch is considered a perennial relatively permanent water (RPW) with a downstream connection to Kankakee River, which is navigable throughout Porter County and located approximately 0.87 river mile southwest of the investigation area. Phillips Ditch would be subject to U.S. Army Corps of Engineers (USACE) jurisdiction under Section 404 of the Clean Water Act due to its connection to a Traditional Navigable Water (TNW) downstream of the project.

Permanent impacts to Phillips Ditch are not anticipated because placement of riprap or any other permanent work will not occur below the OHWM. Temporary impacts to Phillips Ditch are expected to total approximately 140 linear feet (0.07 acre) due to dewatering activities within the stream.

UNT 1 to Keller Ditch

UNT 1 to Keller Ditch is a constructed intermittent stream that flows from north to south and then turns west to head outside the investigation area. This reach of UNT 1 to Keller Ditch has been excavated and channelized to convey agricultural drainage. It has steep, heavily vegetated banks. It is an intermittent stream that is vegetated in portions with a defined bed and bank. Instream vegetation included common duckweed (*Lemna minor*) and narrow leaf cattail (*Typha angustifolia*). Approximately 514 feet of the stream is within the investigated project area, and the stream is located outside the limits of construction. The OHWM is 10 feet wide by 0.5 feet deep. The upstream drainage area is 0.285 square mile. The substrate within this reach of UNT 1 to Keller Ditch is muck (100%).

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UNT 1 to Keller Ditch is considered an intermittent RPW with a downstream connection to the Kankakee River via Keller Ditch. UNT 1 to Keller Ditch would be subject to USACE jurisdiction under Section 404 of the Clean Water Act due to its connection to a TNW downstream of the project.

No permanent or temporary impacts to UNT 1 to Keller Ditch are anticipated due to its location outside the limits of construction. UNT to Keller Ditch will be labeled on the plans as "Do Not Disturb". This is included as a firm commitment in the environmental commitments section of this document.

Due to the 140 linear feet of temporary impacts to Phillips Ditch, an Indiana Department of Environmental Management (IDEM) Section 401 Nationwide Permit (NWP) will likely be required. Because impacts to streams total less than 300 linear feet, mitigation is not anticipated.

The U.S. Coast Guard, 9th District replied on January 17, 2023 that the Coast Guard declines to assert jurisdiction over any waterway in the project area, and that a bridge permit from the Coast Guard is not required (Appendix C, C12).

The USACE responded on January 19, 2023, indicating that water resources may exist at the proposed project site, and that a permit authorizing impacts to Waters of the U.S. may be needed before proceeding with the project (Appendix C, C10 to C11).

Indiana Department of Natural Resources (IDNR) Division of Fish and Wildlife (DFW) responded on February 8, 2023, with recommendations to avoid and minimize impacts to fish, wildlife, and botanical resources, to maintain or improve fish and wildlife passage through any crossing structure, to stabilize stream banks, to impose time of year restrictions on instream work, and to disallow deposition of construction and demolition materials into the waterway (Appendix C, C15 to C17).

All applicable recommendations are included in the *Environmental Commitments* section of this CE document.

Open Water Feature(s)	Presence	Impacts	
		Yes	No
Reservoirs	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Lakes	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Farm Ponds	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Retention/Detention Basin	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Storm Water Management Facilities	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Other: _____	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Describe all open water feature(s) identified adjacent or within the project area. Include whether or not impacts (both permanent and temporary) will occur to the features identified. Include if features are likely subject to federal or state jurisdiction. Discuss measures to avoid, minimize, and mitigate if impacts will occur.

Based on the desktop review, the aerial map of the project area (Appendix B, B3), and the RFI report (Appendix E, E1 to E6), there are two open water feature(s) within the 0.5-mile search radius. There is one water feature adjacent to the project area, which was confirmed by the site visit on August 2, 2022 by Lochmueller Group. The open water feature is located outside the project area and outside the limits of construction and will not be affected by any project activity. Therefore, no impacts are expected.

Wetlands	Presence	Impacts	
		Yes	No
	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Total wetland area: 0.0 Acre(s) Total wetland area impacted: 0.0 Acre(s)

(If a determination has not been made for non-isolated/isolated wetlands, fill in the total wetland area impacted above.)

Wetland No.	Classification	Total Size (Acres)	Impacted Acres	Comments (i.e. location, likely Water of the US, appendix reference)
N/A				
N/A				

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Wetlands (Mark all that apply)

Wetland Determination
 Wetland Delineation
 USACE Isolated Waters Determination

Documentation

X
X

ESD Approval Dates

N/A
N/A

Improvements that will not result in any wetland impacts are not practicable because such avoidance would result in (Mark all that apply and explain):

- Substantial adverse impacts to adjacent homes, business or other improved properties;
- Substantially increased project costs;
- Unique engineering, traffic, maintenance, or safety problems;
- Substantial adverse social, economic, or environmental impacts, or
- The project not meeting the identified needs.

Describe all wetlands identified adjacent or within the project area. Include whether or not impacts (both permanent and temporary) will occur to the features identified. Include if features are likely subject to federal or state jurisdiction. Discuss measures to avoid, minimize, and mitigate if impacts will occur.

Based on the desktop review, the aerial map of the project area (Appendix B, B3), and the RFI report (Appendix E, E1 to E6), there are 15 wetlands within the 0.5-mile search radius. There is one wetland adjacent to the project area. That number was updated to zero by the site visit on August 2, 2022 by Lochmueller Group. Therefore, no impacts are expected.

Terrestrial Habitat

Presence

X

Impacts

Yes	NO
X	

Total terrestrial habitat in project area: 0.44 Acre(s) Total tree clearing: 0.15 Acre(s)

Describe types of terrestrial habitat (i.e. forested, grassland, farmland, lawn, etc) adjacent or within the project area. Include whether or not impacts will occur to habitat identified. Include total terrestrial habitat impacted and total tree clearing that will occur. Discuss measure to avoid, minimize, and mitigate if impacts will occur.

Based on a desktop review, a site visit on August 2, 2022 by Lochmueller Group, and the aerial map of the project area (Appendix B, B3), there are terrestrial habitats within the project area consisting of maintained roadside vegetation and unmaintained riparian areas. Dominant species include tall fescue (*Schedonorus arundinaceus*), Japanese brome (*Bromus japonicus*), and eastern cottonwood (*Populus deltoides*). Approximately 0.44 acre of permanent impacts to terrestrial habitat will occur due to widening of the road and bridge and construction of riprap drainage turnouts. Tree removal totaling approximately 0.15 acre will be required. All efforts to avoid and minimize impacts to terrestrial habitat will occur while meeting the purpose and need of the project. Mitigation is not anticipated.

IDNR DFW responded on February 8, 2023 with recommendations to avoid and minimize impacts to fish, wildlife, and botanical resources, and to develop a mitigation plan for any unavoidable habitat impacts that will occur (Appendix C, C15 to C17). All applicable recommendations are included in the *Environmental Commitments* section of this CE document.

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**Protected Species
Federally Listed Bats**

	Yes	No
Information for Planning and Consultation (IPaC) determination key completed	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Section 7 informal consultation completed (IPaC cannot be completed)	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Section 7 formal consultation Biological Assessment (BA) required	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Determination Received for Listed Bats from USFWS: NE NLAA LAA

Other Species not included in IPaC

	Yes	No
Additional federal species found in project area (based on IPaC species list)	<input checked="" type="checkbox"/>	<input type="checkbox"/>
State species (not bird) found in project area (based upon consultation with IDNR)	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Migratory Birds

	Yes	No
Known usage or presence of birds (i.e. nests)	<input type="checkbox"/>	<input checked="" type="checkbox"/>
State bird species based upon coordination with IDNR	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Discuss IDNR coordination and species identified. Describe USFWS Section 7 consultation and determination received for Indiana bat and northern long-eared bat impacts. Discuss if other federally listed species were identified. If so, include consultation that has occurred and the determination that was received. Discuss if migratory birds have been observed and any impacts.

Based on a desktop review and the RFI report (Appendix E, E1 to E6), completed by Lochmueller Group on October 28, 2022, the IDNR Porter County Endangered, Threatened and Rare (ETR) Species List has been checked. According to the IDNR DFW early coordination response letter dated February 8, 2023 (Appendix C, C15 to C17), the Natural Heritage Program's Database has been checked, and no plant or animal species listed as state or federally threatened, endangered, or rare have been reported to occur in the project vicinity. An INDOT 0.5-mile bat review occurred on May 16, 2022 and did not indicate the presence or endangered bat species or their hibernacula within 0.5 mile of the project area.

Indiana Bat and Northern Long-Eared Bat

Project information was submitted through the US Fish and Wildlife Service's (USFWS) Information for Planning and Consultation (IPaC) portal, and an official species list was generated (Appendix C, C18 to C33). The project is within range of the federally endangered Indiana bat (*Myotis sodalis*) and the federally endangered northern long-eared bat (NLEB) (*Myotis septentrionalis*). Other species were generated in the IPaC species list along with the Indiana bat and NLEB. Refer to the paragraph below.

The project qualifies for the Range-wide Programmatic Informal Consultation for the Indiana bat and northern long-eared bat (NLEB), dated May 2016 (revised February 2018), between FHWA, Federal Railroad Administration (FRA), Federal Transit Administration (FTA), and USFWS. A bridge inspection occurred on August 2, 2022 and found no evidence of bats or birds using the structure (Appendix C, C48). USFWS Bridge/Structure Assessments are only valid for two years. If construction will begin after August 2, 2024, an inspection of the structure by a qualified individual, must be performed. Inspection of the structure should check for presence of bats/bat indicators and/or presence of birds. The results of the inspection must indicate no signs of bats or birds. If signs of bats or birds are documented during this inspection, the INDOT District Environmental Manager must be contacted immediately. This firm commitment is included in the *Environmental Commitments* section of this document. An effect determination key was completed on December 21, 2022, and based on the responses provided, the project was found to "May Affect/Not Likely to Adversely Affect (MA/NLAA)" the Indiana bat and/or the NLEB (Appendix C, C34 to C47). INDOT reviewed and verified the effect finding on October 21, 2022 and requested USFWS's review of the finding. No response was received from USFWS within the 14-day review period; therefore, it was concluded they concur with the finding. Avoidance and Minimization Measures (AMMs) for this project included General AMM 1, Lighting AMM 1, and Tree Removal AMMs 1 through 4, which include measures pertaining to bat habitat awareness, use of temporary lighting, and tree clearing considerations and restrictions. AMMs are included as firm commitments in the *Environmental Commitments* section of this document.

The official species list generated from IPaC indicated three other species present within the project, including the tricolored bat (*Perimyotis subflavus*, proposed endangered), whooping crane (*Grus americana*, experimental population, non-essential) and monarch butterfly (*Danaus plexippus*, candidate). The tricolored bat is listed as proposed endangered and was preemptively evaluated assuming the species will be listed as endangered before this project is completed. The whooping crane is listed as endangered wherever found, except where listed as an experimental population according to the Environmental Conservation Online System. The whooping crane is listed as an experimental population in this location. The monarch butterfly is identified as a candidate species, which is not yet listed or proposed for listing. Therefore, these species were not considered as part of this project. The project qualifies for the most current INDOT/USFWS agreement. No further coordination is needed with the USFWS.

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Migratory Birds

Structure 64-00066 (41.295570, -87.112725) and the project's surrounding habitat is conducive for use (i.e. nests) by a bird species protected under the Migratory Bird Treaty Act (MBTA). Prior to the start of nesting season (May 1) the structure must be inspected for birds or signs of birds. If birds or signs of birds are found during the inspection avoidance and minimization measures must be implemented prior to the start of and during the nesting season. Nests without eggs or young should be removed prior to construction during the non-nesting season (September 8 – April 30) and during the nesting season if no eggs or young are present. Nests with eggs or young cannot be removed or disturbed during the nesting season (May 1 – September 7). Nests with eggs or young should be screened or buffered from active construction. Details of the required procedures are outlined in the Recurring Special Provision (RSP) 107-C-273 "Migratory Bird Protection." This firm commitment is included in the *Environmental Commitments* of this document.

This precludes the need for further consultation on this project as required under Section 7 of the Endangered Species Act, as amended. If new information on endangered species at the site becomes available, or if project plans are changed, USFWS will be contacted for consultation.

Geological and Mineral Resources

- Project located within the Indiana Karst Region
- Karst features identified within or adjacent to the project area
- Oil/gas or exploration/abandoned wells identified in the project area

Yes	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>

Date Karst Evaluation reviewed by INDOT EWPO (if applicable): N/A

Discuss if project is located in the Indiana Karst Region and if any karst features have been identified in the project area (from RFI). Discuss response received from IGWS coordination. Discuss if any mines, oil/gas, or exploration/abandoned wells were identified and if impacts will occur. Include discussion of karst study/report was completed and results. (Karst investigation must comply with the current Protection of Karst Features during Planning and Construction guidance and coordinated and reviewed by INDOT EWPO)

Based on a desktop review and the Indiana Karst Region map, the project is located outside the designated Indiana Karst Region as outlined in the most current *Protection of Karst Features during Project Development and Construction*. According to the topographic map of the project area (Appendix B, B2) the RFI report (Appendix E, E1 to E6), there are no karst features identified within or adjacent to the project area. In the early coordination response dated January 10, 2023, the Indiana Geological and Water Survey (IGWS) did not indicate that karst features exist in the project area (Appendix C, C5 to C6). IGWS noted the site contained high liquefaction potential, 1% annual chance flood hazard, low potential for bedrock resource, and high potential for sand and gravel resource. The response from IGWS has been communicated with the designer on February 10, 2023. No impacts are expected.

SECTION C – OTHER RESOURCES

Drinking Water Resources

- Wellhead Protection Area(s)
- Source Water Protection Area(s)
- Water Well(s)
- Urbanized Area Boundary
- Public Water System(s)

Presence

<input type="checkbox"/>
<input type="checkbox"/>
<input checked="" type="checkbox"/>
<input checked="" type="checkbox"/>
<input type="checkbox"/>

Impacts

Yes	No
<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>

- Is the project located in the St. Joseph Sole Source Aquifer (SSA):
- If Yes, is the FHWA/EPA SSA MOU Applicable?
- If Yes, is a Groundwater Assessment Required?

Yes	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>

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Check the appropriate boxes and discuss each topic below. Provide details about impacts and summarize resource-specific coordination responses and any mitigation commitments. Reference responses in the Appendix.

Sole Source Aquifer

The project is located in Porter County, which is not located within the area of the St. Joseph Sole Source Aquifer, the only legally designated sole source aquifer in the state of Indiana. Therefore, the FHWA/Environmental Protection Agency (EPA)/INDOT Sole Source Aquifer Memorandum of Understanding (MOU) is not applicable to this project, a detailed groundwater assessment is not needed, and no impacts are expected.

Wellhead Protection Area and Source Water

The Indiana Department of Environmental Management's Wellhead Proximity Determinator website (<http://www.in.gov/idem/cleanwater/pages/wellhead/>) was accessed on January 10, 2023 by Lochmueller Group. This project is not located within a Wellhead Protection Area or Source Water Area. No impacts are expected.

Water Wells

The IDNR Water Well Record Database website (<https://www.in.gov/dnr/water/3595.htm>) was accessed on January 10, 2023 by Lochmueller Group. A well is present near the project area. The well will not be affected because it is located outside the project construction limits, and work will not occur within the vicinity of the well. Therefore, no impacts are expected. Should it be determined during the right-of-way phase that these wells will be affected, a cost to cure will likely be included in the appraisal to restore the wells. This is a firm commitment and included in the Environmental Commitments section of this document.

Urban Area Boundary

Based on a desktop review of the IDEM Multiple Separate Storm Sewer Systems (MS4) boundary map (<https://www.in.gov/idem/cleanwater/ms4s-boundaries-map-for-indiana/>) by Lochmueller Group on January 10, 2023, this project is located in an Urban Area Boundary (UAB). An early coordination letter was sent on January 10, 2023, to the Porter County MS4 Coordinator. The MS4 coordinator did not respond within the 30-day time frame. The project will comply with an approved storm water quality management plan.

Public Water System

Based on a desktop review, a site visit on August 2, 2022 by Lochmueller Group, the aerial map of the project area (Appendix B, B3), and the project plans (Appendix B, B12 to B24), no public water systems were identified. Therefore, no impacts are expected.

Floodplains

- Project located within a regulated floodplain
- Longitudinal encroachment
- Transverse encroachment
- Homes located in floodplain within 1000' up/downstream from project

Presence

X
X
X

Impacts

Yes	No
X	
X	
	X

If applicable, indicate the Floodplain Level?

Level 1 Level 2 Level 3 Level 4 Level 5

Use the IDNR Floodway Information Portal to help determine potential impacts. Include floodplain map in appendix. Discuss impacts according to the classification system. If encroachment on a flood plain will occur, coordinate with the Local Flood Plain Administrator during design to insure consistency with the local flood plain planning.

Based on a desktop review of The Indiana Department of Natural Resources Indiana Floodway Information Portal website (<https://indnr.maps.arcgis.com/apps/webappviewer/index.html?id=05026dabc2e8461983e196d56a213c1e>) by Lochmueller Group on December 14, 2022, and the RFI report (Appendix E, E1 to E6), this project is located in a regulatory floodplain as determined from approved IDNR floodplain maps (Appendix F, F18). An early coordination letter was sent on January 10, 2023, to the Porter County Floodplain Administrator. The floodplain administrator did not respond within the 30-day time frame. The IDNR DFW responded on February 8, 2023 indicating that the project will require formal approval of IDNR for construction in a floodway pursuant to the Flood Control Act (AC 14-28-1), unless it qualifies for a bridge exemption (Appendix C, C15 to C17). The project will not likely qualify for the rural bridge exemption due to the presence of a home in the floodway within 1,000 feet upstream of the project.

This project qualifies as a Category 4 per the current INDOT CE Manual, which states: One home is located within the base floodplain within 1,000 feet upstream and no homes are located within the base floodplain within 1,000 feet downstream. The

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proposed structure will have an effective capacity such that backwater surface elevations are not expected to substantially increase. As a result, there will be no substantial adverse impacts on natural and beneficial floodplain values; there will be no substantial change in flood risks; and there will be no substantial increase in potential for interruption or termination of emergency service or emergency evacuation routes; therefore, it has been determined that this encroachment is not substantial.

Farmland	Presence	Impacts	
		Yes	No
Agricultural Lands	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Prime Farmland (per NRCS)	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
 Total Points (from Section VII of CPA-106/AD-1006*)	 149		

**If 160 or greater, see CE Manual for guidance.*

Discuss existing farmland resources in the project area, impacts that will occur to farmland, and mitigation and minimization measures considered.

Based on a desktop review, a site visit on August 2, 2022 by Lochmueller Group and the aerial map of the project area (Appendix B, B3), the project will convert 0.66 acre of farmland as defined by the Farmland Protection Policy Act. An early coordination letter was sent on January 10, 2023, to U.S. Department of Agriculture (USDA) Natural Resources Conservation Service (NRCS). Coordination with NRCS resulted in a score of 149 on the AD 1006 Form (Appendix C, C14). NRCS's threshold score for significant impacts to farmland that result in the consideration of alternatives is 160. Since this project score is less than the threshold, no significant loss of prime, unique, statewide, or local important farmland will result from this project. No alternatives other than those previously discussed in this document will be investigated without reevaluating impacts to prime farmland.

SECTION D – CULTURAL RESOURCES

Minor Projects PA	Category(ies) and Type(s) <input type="text" value="B-12, A-4, A-6, A-9"/>	INDOT Approval Date(s) <input type="text" value="July 31, 2023"/>	N/A <input type="text"/>
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Full 106 Effect Finding

No Historic Properties Affected No Adverse Effect Adverse Effect

Eligible and/or Listed Resources Present

NRHP Building/Site/District(s) Archaeology NRHP Bridge(s)

Documentation Prepared (mark all that apply)	ESD Approval Date(s)	SHPO Approval Date(s)
APE, Eligibility and Effect Determination	<input type="checkbox"/>	<input type="checkbox"/>
800.11 Documentation	<input type="checkbox"/>	<input type="checkbox"/>
Historic Properties Report or Short Report	<input type="checkbox"/>	<input type="checkbox"/>
Archaeological Records Check and Assessment	<input type="checkbox"/>	<input type="checkbox"/>
Archaeological Phase Ia Survey Report	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Archaeological Phase Ic Survey Report	<input type="checkbox"/>	<input type="checkbox"/>
Other:	<input type="checkbox"/>	<input type="checkbox"/>

Memorandum of Agreement (MOA) **MOA Signature Dates** (List all signatories)

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If the project falls under the MPPA, describe the category(ies) that the project falls under and any approval dates. If the project requires full Section 106, use the headings provided. The completion of the Section 106 process requires that a Legal Notice be published in local newspapers. Please indicate the publication date, name of the paper(s) and the comment period deadline. Include any further Section 106 work which must be completed at a later date, such as mitigation from a MOA or avoidance commitments.

On July 31, 2023 the INDOT Cultural Resources Office (CRO) determined that this project falls within the guidelines of Category B, Type B-12 and Category A, Types A-4, A-6, and A-9 under the Minor Projects Programmatic Agreement (MPPA), (Appendix D, D1 to D7). The MPPA Types include the following work:

- B-12: Replacement, widening, or raising the elevation of the superstructure on existing bridges, and bridge replacement projects (when both the superstructure and substructure are removed).
- A-4: Roadway work associated with surface replacement, reconstruction, rehabilitation, or resurfacing projects, including overlays, shoulder treatments, pavement repair, seal coating, pavement grinding, and pavement marking within previously disturbed soils where replacement, repair, or installation of curbs, curb ramps or sidewalks will not be required.
- A-6: Repair, replacement, or upgrade of existing safety appurtenances such as guardrails, barriers, glare screens, and crash attenuators in previously disturbed soils.
- A-9: Installation, repair, or replacement of erosion control measures along roadways, waterways and bridge piers within previously disturbed soils.

Because the project takes place within previously undisturbed soils, a Phase Ia Archaeological Survey was required. A records review was conducted, and a Phase Ia survey was conducted on July 27 and July 28, 2022. The survey area covered 6.1 acres and included a pedestrian survey, shovel testing, and a visual inspection. The reconnaissance resulted in the identification of one previously unrecorded archaeological site, which was recommended not eligible for inclusion in the National Register of Historic Places. No further archaeological work is recommended for the project.

No further consultation is required. This completes the Section 106 process and the responsibilities of the FHWA under Section 106 have been fulfilled.

SECTION E – SECTION 4(f) RESOURCES/ SECTION 6(f) RESOURCES

	<u>Presence</u>	<u>Use</u>	
		<u>Yes</u>	<u>No</u>
Parks and Other Recreational Land			
Publicly owned park			
Publicly owned recreation area			
Other (school, state/national forest, bikeway, etc.)			
Wildlife and Waterfowl Refuges			
National Wildlife Refuge			
National Natural Landmark			
State Wildlife Area			
State Nature Preserve			
Historic Properties			
Site eligible and/or listed on the NRHP			
<u>Evaluations</u>			
<u>Prepared</u>			
Programmatic Section 4(f)			
“De minimis” Impact			
Individual Section 4(f)			
Any exception included in 23 CFR 774.13			

Discuss Programmatic Section 4(f) and “de minimis” Section 4(f) impacts in the discussion below. Individual Section 4(f) documentation must be included in the appendix and summarized below. Discuss proposed alternatives that satisfy the requirements of Section 4(f). FHWA has identified various exceptions to the requirement for Section 4(f) approval. Refer to 23 CFR § 774.13 - Exceptions.

Section 4(f) of the U.S. Department of Transportation Act of 1966 prohibits the use of certain public and historic lands for federally

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funded transportation facilities unless there is no feasible and prudent alternative. The law applies to significant publicly owned parks, recreation areas, wildlife / waterfowl refuges, and NRHP eligible or listed historic properties regardless of ownership. Lands subject to this law are considered Section 4(f) resources.

Based on a desktop review, the aerial map of the project area (Appendix B, B3), and the RFI report (Appendix E, E1 to E6) there are no potential 4(f) resources located within the 0.5-mile search radius. According to additional research and by the site visit on August 2, 2022 by Lochmueller Group, there are no Section 4(f) resources within or adjacent to the project area. Therefore, no use is expected.

Section 6(f) Involvement

Presence

Use

Section 6(f) Property

Yes

No

Discuss Section 6(f) resources present or not present. Discuss if any conversion would occur as a result of this project. If conversion will occur, discuss the conversion approval.

The U.S. Land and Water Conservation Fund Act of 1965 established the Land and Water Conservation Fund (LWCF), which was created to preserve, develop, and assure accessibility to outdoor recreation resources. Section 6(f) of this Act prohibits conversion of lands purchased with LWCF monies to a non-recreation use.

A review of 6(f) properties on the INDOT ESD website revealed a total of 31 properties in Porter County (Appendix I, I1). None of these properties are located within or adjacent to the project area. Therefore, there will be no impacts to 6(f) resources.

SECTION F – Air Quality

STIP/TIP and Conformity Status of the Project

- Is the project in the most current STIP/TIP?
- Is the project located in an MPO Area?
- Is the project in an air quality non-attainment or maintenance area?
- If Yes, then:
 - Is the project in the most current MPO TIP?
 - Is the project exempt from conformity?
- If No, then:
 - Is the project in the Transportation Plan (TP)?
 - Is a hot spot analysis required (CO/PM)?

Yes	No
<input checked="" type="checkbox"/>	<input type="checkbox"/>
<input checked="" type="checkbox"/>	<input type="checkbox"/>
<input checked="" type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>
<input checked="" type="checkbox"/>	<input type="checkbox"/>
<input checked="" type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>

Location in STIP:

By Reference of TIP

Name of MPO (if applicable):

Northwestern Indiana Regional Planning Commission (NIRPC)

Location in TIP (if applicable):

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Level of MSAT Analysis required?

Level 1a Level 1b Level 2 Level 3 Level 4 Level 5

Describe if the project is listed in the STIP and if it is in a TIP. Describe the attainment status of the county(ies) where the project is located. Indicate whether the project is exempt from a conformity determination. If the project is not exempt, include information about the TP and TIP. Describe if a hot spot analysis is required and the MSAT Level.

STIP/TIP

This project is included in the Fiscal Year (FY) 2022 to 2026 Northwestern Indiana Regional Planning Commission (NIRPC) Transportation Improvement Program (TIP) via Amendment 22-09, which was directly incorporated into the INDOT 2024 to 2028 Statewide Transportation Improvement Program (STIP) (Appendix H, H1 to H5).

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Attainment Status

This project is located in Porter County, which is currently a nonattainment area for Ozone under the 2015 8-Hour Standard and a maintenance area for Ozone under the 2008 8-Hour Standard and the 1997 8-Hour Standard. The 1997 8-hour standard was revoked in 2015 but is being evaluated for conformity due to the February 16, 2018, South Coast Air Quality Management District V. Environmental Protection Agency, et. al. decision. This project has been identified as being exempt from air quality analysis in accordance with 40 CFR Part 93.126 and this project is not a project of air quality concern (40 CFR Part 93.123). Therefore, the project will have no significant impact on air quality.

MSAT

This project is of a type qualifying as a categorical exclusion (Group 1) under 23 CFR 771.117(c), or exempt under the Clean Air Act conformity rule under 40 CFR 93.126, and as such, a Mobile Source Air Toxics analysis is not required.

SECTION G - NOISE

Noise

Yes No

Is a noise analysis required in accordance with FHWA regulations and INDOT's traffic noise policy?

Date Noise Analysis was approved/technically sufficient by INDOT ESD: N/A

Describe if the project is a Type I or Type III project. If it is a Type I project, describe the studies completed to date and if noise impacts were identified. If noise impacts were identified, describe if abatement is feasible and reasonable and include a statement of likelihood.

This project is a Type III project. In accordance with 23 CFR 772 and the current Indiana Department of Transportation Traffic Noise Analysis Procedure, this action does not require a formal noise analysis.

SECTION H – COMMUNITY IMPACTS

Regional, Community & Neighborhood Factors

Yes No

Will the proposed action comply with the local/regional development patterns for the area?

Will the proposed action result in substantial impacts to community cohesion?

Will the proposed action result in substantial impacts to local tax base or property values?

Will construction activities impact community events (festivals, fairs, etc.)?

Does the community have an approved transition plan?

If No, are steps being made to advance the community's transition plan?

Does the project comply with the transition plan? (explain in the discussion below)

Discuss how the project complies with the area's local/regional development patterns; whether the project will impact community cohesion; and impact community events. Discuss how the project conforms with the ADA Transition Plan.

The project complies with local and regional development patterns and will have a negligible impact on community cohesion and community events. The bridge replacement will have a positive community impact by elongating the life and addressing deficiencies of the crossing while gaining greater ability to handle growing transportation needs of the community. ROW acquisition along the property frontage will not substantially diminish the affected properties' existing use.

According to the Indiana Festivals website (<https://indianafestivals.org/>) and the Town of Kouts website (<http://www.koutsindiana.org/town-calendar/>), accessed on February 9, 2023 by Lochmueller Group, there are no festivals occurring nearby at the time of construction.

The project complies with Porter County's December 2012 Americans with Disabilities Act (ADA) Transition Plan. The project will not infringe on people with disabilities' ability to participate or enjoy community programs, services, or activities. No sidewalks or buildings warranting ADA compliance are present within the project area or planned as part of the project.

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Public Facilities and Services

Discuss what public facilities and services are present in the project area and impacts (such as MOT) that will occur to them. Include how the impacts have been minimized and what coordination has occurred. Some examples of public facilities and services include health facilities, educational facilities, public and private utilities, emergency services, religious institutions, airports, transportation or public pedestrian and bicycle facilities.

Based on a desktop review, the aerial map of the project area (Appendix B, B3), and the RFI report (Appendix E, E1 to E6) there are no public facilities within the 0.5-mile search radius. There are no public facilities within or adjacent to the project area, which was confirmed by the site visit on August 2, 2022 by Lochmueller Group. Therefore, no impacts are expected. Access to all properties will be maintained during construction.

The Transportation Director of the Metropolitan School District of Boone Township responded to early coordination efforts via phone call on January 10, 2023. The Transportation Director indicated that closure of Bridge No. 66 would interfere with an existing bus route, adding an additional 10 to 15 minutes of travel time and requiring the bus to turn around after picking up a student approximately 0.1 mile south of the bridge. The Transportation Director requested the MOT be made to overlap the summer out-of-school session to the extent possible. Lochmueller Group provided the telephone record to the Transportation Director, who approved the document on January 13, 2023. Lochmueller Group notified the project design team of the issues for consideration into the MOT plan. Coordination with Metropolitan School District of Boone Township will continue as the project develops. This action is a firm commitment under Firm Commitment No. 2 in the *Commitments* section of this CE document.

It is the responsibility of the project sponsor to notify school corporations and emergency services at least two weeks prior to any construction that would block or limit access. This is a firm commitment and included in the *Environmental Commitments* section of this document.

Environmental Justice (EJ) (Presidential EO 12898)

During the development of the project were EJ issues identified?

Does the project require an EJ analysis?

If YES, then:

Are any EJ populations located within the project area?

Will the project result in adversely high and disproportionate impacts to EJ populations?

Yes	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input checked="" type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>

Indicate if EJ issues were identified during project development. If an EJ analysis was not required, discuss why. If an EJ analysis was required, describe how the EJ population was identified. Include if the project has a disproportionately high or adverse effect on EJ populations and explain your reasoning. If yes, describe actions to avoid, minimize and mitigate these effects.

Under FHWA Order 6640.23A, FHWA and the project sponsor, as a recipient of funding from FHWA, are responsible to ensure that their programs, policies, and activities do not have a disproportionately high and adverse effect on minority or low-income populations. Per the current INDOT Categorical Exclusion Manual, an Environmental Justice (EJ) Analysis is required for any project that has two or more relocations or 0.5 acre of additional permanent right-of-way. The project will require 0.87 acre of new permanent ROW, and no relocations. Therefore, an EJ Analysis is required.

Potential EJ impacts are detected by locating minority and low-income populations relative to a reference population to determine if populations of EJ concern exists and whether there could be disproportionately high and adverse impacts to them. The reference population may be a county, city or town and is called the community of comparison (COC). In this project, the COC is Porter County. The community that overlaps the project area is called the affected community (AC). In this project, the AC is Census Tract 511.01. An AC has a population of concern for EJ if the population is more than 50% minority or low-income or if the low-income or minority population is 125% of the COC. Data from the US Census Bureau webpage (<https://data.census.gov/cedsci/>) was obtained on February 3, 2023 by Lochmueller Group. The data collected for minority and low-income populations within the AC are summarized in the below table.

Indiana Department of Transportation

County Porter

Route CR 250 W

Des. No. 2003041

Table: Minority and Low-Income Data (US Census Bureau, 2021 5-Year Estimates)		
	COC – Porter County	AC - Census Tract 511.01
Percent Minority	18.3%	8.2%
125% of COC	22.9%	8.2% < 22.9%
EJ Population of Concern		No
Percent Low-Income	10.1%	6.2%
125% of COC	12.6%	6.2% < 12.6%
EJ Population of Concern		No

The AC, Census Tract 511.01, has a percent minority of 8.2% which is below 50% and is below the 125% COC threshold. Therefore, the AC does not contain minority populations of EJ concern.

The AC, Census Tract 511.01, has a percent low-income of 6.2% which is below 50% and is below the 125% COC threshold. Therefore, the AC does not contain low-income populations of EJ concern.

Conclusion

The census data sheets, map, and calculations can be found in Appendix I, I11 to I19. The AC does not contain minority or low-income populations of EJ concern. The project will benefit communities in this area due to the improved condition at the crossing. It is expected that the project will not have a disproportionately high and adverse environmental or health impact to low-income or minority populations of EJ concern when compared to non-EJ populations. No further environmental justice analysis is warranted.

Relocation of People, Businesses or Farms

Will the proposed action result in the relocation of people, businesses or farms?
Is a BIS or CSRS required?

Yes	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>

Number of relocations: Residences: 0 Businesses: 0 Farms: 0 Other: 0

Discuss any relocations that will occur due to the project. If a BIS or CSRS is required, discuss the results in the discussion below.

No relocations of people, businesses, or farms will take place as a result of this project.

SECTION I – HAZARDOUS MATERIALS & REGULATED SUBSTANCES

Hazardous Materials & Regulated Substances (Mark all that apply)

- Red Flag Investigation (RFI)
- Phase I Environmental Site Assessment (Phase I ESA)
- Phase II Environmental Site Assessment (Phase II ESA)
- Design/Specifications for Remediation required?

Documentation

X

Date RFI concurrence by INDOT SAM (if applicable): October 31, 2022

Include a summary of the potential hazardous material concerns found during review. Discuss in depth sites found within, directly adjacent to, or ones that could impact the project area. Refer to current INDOT SAM guidance. If additional documentation (special provisions, pay quantities, etc.) will be needed, include in discussion. Include applicable commitments.

Based on a review of Geographic Information Systems (GIS) and available public records, the RFI was completed on October 28, 2022 by Lochmueller Group and INDOT Site Assessment and Management (SAM) provided their concurrence on October 31, 2022

Indiana Department of Transportation

County Porter

Route CR 250 W

Des. No. 2003041

(Appendix E, E1 to E6). No sites with hazardous material concerns (hazmat sites) or sites involved with regulated substances were identified in or within 0.5 mile of the project area. Further investigation for hazardous material concerns or regulated substances is not required at this time.

Part IV – Permits and Commitments

PERMITS CHECKLIST

Permits (mark all that apply)

Likely Required

Army Corps of Engineers (404/Section10 Permit)

Nationwide Permit (NWP)	<input type="checkbox"/>
Regional General Permit (RGP)	<input type="checkbox"/>
Individual Permit (IP)	<input type="checkbox"/>
Other	<input type="checkbox"/>

IN Department of Environmental Management (401/Rule 5)

Nationwide Permit (NWP)	<input checked="" type="checkbox"/>
Regional General Permit (RGP)	<input type="checkbox"/>
Individual Permit (IP)	<input type="checkbox"/>
Isolated Wetlands	<input type="checkbox"/>
Rule 5	<input type="checkbox"/>
Other	<input type="checkbox"/>

IN Department of Natural Resources

Construction in a Floodway	<input checked="" type="checkbox"/>
Navigable Waterway Permit	<input type="checkbox"/>
Other	<input type="checkbox"/>

Mitigation Required

US Coast Guard Section 9 Bridge Permit

Others (Please discuss in the discussion below)

List the permits likely required for the project and summarize why the permits are needed, including permits designated as "Other."

Due to the 140 linear feet (0.07 acre) of temporary impacts to Phillips Ditch, an IDEM Section 401 NWP will likely be required.

Due to construction occurring within the regulated floodway of Phillips Ditch, a Construction in a Floodway permit from IDNR will likely be required.

Applicable recommendations provided by resource agencies are included in the Environmental Commitments section of this document. If permits are found to be necessary, the conditions of the permit will be the requirements of the project and will supersede these recommendations.

It is the responsibility of the project sponsor to identify and obtain all required permits.

ENVIRONMENTAL COMMITMENTS

List all commitments and include the name of agency/organization requesting/requiring the commitment(s). Listed commitments should be numbered.

Firm:

- If the scope of work or permanent or temporary right-of-way amounts change, the INDOT Environmental Services Division (ESD) and the INDOT District Environmental Section will be contacted immediately. (INDOT ESD and INDOT LaPorte

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Route CR 250 W

Des. No. 2003041

District)

2. It is the responsibility of the project sponsor to notify school corporations and emergency services at least two weeks prior to any construction that would block or limit access. (INDOT ESD)
3. Any work in a wetland area within right-of-way or in borrow/waste areas is prohibited unless specifically allowed in the U.S. Army Corps of Engineers permit. (INDOT EWPO)
4. Coordination with the Area Engineer will occur during contract preparation to ensure a summer timeframe for the detour is provided.
5. Phillips Ditch is listed as impaired for E. coli. Workers who are working in or near water with E. coli should take care to wear appropriate PPE, observe proper hygiene procedures, including regular hand washing, and limit personal exposure. (INDOT SAM)
6. USFWS Bridge/Structure Assessment for Structure No. 64-00001 shall take place no earlier than two (2) years prior to the start of construction. If construction will begin after August 2, 2024 an inspection of the structure by a qualified individual, must be performed. Inspection of the structure should check for presence of bats/bat indicators and/or presence of birds. The results of the inspection must indicate no signs of bats or birds. If signs of bats or birds are documented during this inspection, the INDOT District Environmental Manager must be contacted immediately. (INDOT ESD)
7. Structure 64-00066 (41.295570, -87.112725) and the project's surrounding habitat is conducive for use (i.e. nests) by a bird species protected under the Migratory Bird Treaty Act (MBTA). Prior to the start of nesting season (May 1) the structure must be inspected for birds or signs of birds. If birds or signs of birds are found during the inspection avoidance and minimization measures must be implemented prior to the start of and during the nesting season. Nests without eggs or young should be removed prior to construction during the non-nesting season (September 8 – April 30) and during the nesting season if no eggs or young are present. Nests with eggs or young cannot be removed or disturbed during the nesting season (May 1 – September 7). Nests with eggs or young should be screened or buffered from active construction. Details of the required procedures are outlined in the Recurring Special Provision (RSP) 107-C-273 "Migratory Bird Protection." (INDOT ESD)
8. Should it be determined during the right-of-way phase that wells will be affected, a cost to cure will likely be included in the appraisal to restore the wells. (INDOT ROW)
9. General AMM 1: Ensure all operators, employees, and contractors working in areas of known or presumed bat habitat are aware of all FHWA/FRA/FTA (Transportation Agencies) environmental commitments, including all applicable AMMs. (USFWS)
10. Lighting AMM 1: Direct temporary lighting away from suitable habitat during the active season. (USFWS)
11. Tree Removal AMM 1: Modify all phases/aspects of the project (e.g., temporary work areas, alignments) to avoid tree removal. (USFWS)
12. Tree Removal AMM 2: Apply time of year restrictions (October 1st through March 31st) for tree removal when bats are not likely to be present, or limit tree removal to 10 or fewer trees per project at any time of year within 100 feet of existing road/rail surface and outside of documented roosting/foraging habitat or travel corridors; visual emergence survey must be conducted with no bats observed. (USFWS)
13. Tree Removal AMM 3: Ensure tree removal is limited to that specified in project plans and ensure that contractors understand clearing limits and how they are marked in the field (e.g., install bright colored flagging/fencing prior to any tree clearing to ensure contractors stay within clearing limits). (USFWS)
14. Tree Removal AMM 4: Do not remove documented Indiana bat or NLEB roosts that are still suitable for roosting, or trees within 0.25 miles of roosts, or documented foraging habitat any time of year. (USFWS)
15. UNT to Keller Ditch will be labeled on the plans as "Do Not Disturb". (INDOT ESD)

For Further Consideration:

1. Riprap must not be placed in the active thalweg channel or placed in the streambed in a manner that precludes fish or aquatic organism passage (riprap must not be placed above the existing streambed elevation). Riprap may be used only at the toe of the sideslopes up to the ordinary high water mark (OHWM). The banks above the OHWM must be restored, stabilized, and revegetated using geotextiles and a mixture of grasses, sedges, wildflowers, shrubs, and trees native to Northern Indiana and specifically for stream bank/floodway stabilization purposes as soon as possible upon completion. (IDNR DFW)
2. Impacts to non-wetland forest of one (1) acre or more should be mitigated at a minimum 2:1 ratio. If less than one acre of nonwetland forest is removed in a rural setting, replacement should be at a 1:1 ratio based on area. Impacts to nonwetland forest under one (1) acre in an urban setting should be mitigated by planting five trees, at least 2 inches in diameter-at-breast height (dbh), for each tree which is removed that is 10 inches dbh or greater (5:1 mitigation based on the number of large trees). (IDNR DFW)
3. Operate equipment used to replace the bridge from the existing roadway. (IDNR DFW)
4. Use minimum average 6 inch graded riprap stone extended below the normal water level to provide habitat for aquatic organisms in the voids. (IDNR DFW)
5. Do not cut any trees suitable for Indiana bat or Northern Long-eared bat roosting (greater than 5 inches dbh, living or dead, with loose hanging bark, or with cracks, crevices, or cavities) from April 1 through September 30. (IDNR DFW)

Indiana Department of Transportation

County Porter

Route CR 250 W

Des. No. 2003041

- 6. The new, replacement, or rehabbed structure should not create conditions that are less favorable for wildlife passage under the structure compared to current conditions. (IDNR DFW)
- 7. Do not work in the waterway from April 1 through June 30 without prior written approval of the Division of Fish and Wildlife. (IDNR DFW).

Appendix A: INDOT Supporting Documentation

Threshold Chart.....A1

Appendix B: Graphics

General Location Map..... B1
Topographic Map (1:24,000) B2
Aerial Map B3
Photo Location Map B4
Site Photographs B5-B12
Preliminary Plans..... B13-B24

Appendix C: Early Coordination

Sample Early Coordination Letter (January 10, 2023)..... C1-C4
Indiana Geological and Water Survey
Automatic Response Letter (January 10, 2023) C5-C6
Metropolitan School District of Boone Township
Telephone Record (January 10, 2023) C7
Follow-up Email (January 13, 2023)..... C8
INDOT LaPorte District
Response Letter (January 12, 2023)..... C9
US Army Corps of Engineers, Chicago District
Response Letters (January 12 & January 19, 2023)..... C10-C11
US Department of Homeland Security, US Coast Guard, Ninth District
Response Letter (January 17, 2023)..... C12
US Department of Agriculture, Natural Resource Conservation Service
Response Letter (January 19, 2023) C13
AD-1006 Form C14
Indiana Department of Natural Resources, Division of Fish and Wildlife
Response Letter (February 8, 2023)..... C15-C17
US Department of the Interior, Fish and Wildlife Service
Official Species List (December 20, 2022)..... C18-C33
Concurrence Verification Letter (December 21, 2022)..... C34-C47
Bridge/Structure Bat Assessment Forms (August 2, 2022)..... C48

Appendix D: Section 106 of the National Historic Preservation Act (NHPA)

Minor Projects PA Project Assessment Form (Approved July 31, 2023) D1-D7
Phase 1A Archaeological Report Excerpt D8-D9

Appendix E: Red Flag Investigation and Hazardous Materials

Red Flag Investigation (Approved October 31, 2022) E1-E6

Appendix F: Water Resources

Waters of the US Report (September 16, 2022) F1-F5
Location Map F6
USGS Quad Map (1:24,000)..... F7
USGS Quad Map (1:12,000)..... F8
USDA SSURGO Soils Map..... F9
USFWS NWI Map F10
StreamStats Watershed Map F11
Best Available Flood Hazard & NHD Line Map F12
Water Resources Map F13
Preliminary Jurisdictional Determination (PJD) Form F14-F17
IDNR Floodplain Analysis & Regulatory Assessment (FARA) Report F18

Appendix G: Public Involvement

Sample Notice of Entry Letter (February 25, 2022).....G1

Appendix H: Air Quality

NIRPC 2022-2026 TIP Amendment 22-09H1
2024-2028 STIP Plan Letter H2-H3
2024-2028 STIP Approval Letter H4-H5

Appendix I: Other Information

Land and Water Conservation Fund Grants: Porter County I1
Bridge Inspection Report (May 23, 2022).....I2-I10
Environmental JusticeI11-I19

Categorical Exclusion

Appendix A

INDOT Supporting Documentation

Categorical Exclusion Level Thresholds

	PCE	Level 1	Level 2	Level 3	Level 4 ¹
Section 106	Falls within guidelines of Minor Projects PA	“No Historic Properties Affected”	“No Adverse Effect”	-	“Adverse Effect” Or Historic Bridge involvement ²
Stream Impacts³	No construction in waterways or water bodies	< 300 linear feet of stream impacts	≥ 300 linear feet of stream impacts	-	USACE Individual 404 Permit ⁴
Wetland Impacts³	No adverse impacts to wetlands	< 0.1 acre	-	< 1.0 acre	≥ 1.0 acre
Right-of-way⁵	Property acquisition for preservation only or none	< 0.5 acre	≥ 0.5 acre	-	-
Relocations	None	-	-	< 5	≥ 5
Threatened/Endangered Species (Species Specific Programmatic for Indiana bat & northern long eared bat)*	“No Effect”, “Not likely to Adversely Affect” (With select AMMs ⁶)	“Not likely to Adversely Affect” (With any AMMs or commitments)	-	“Likely to Adversely Affect”	Project does not fall under Species Specific Programmatic ⁷
Threatened/Endangered Species (Any other species)*	Falls within guidelines of USFWS 2013 Interim Policy or “No Effect”	“Not likely to Adversely Affect”	-	-	“Likely to Adversely Affect”
Environmental Justice	No disproportionately high and adverse impacts	-	-	-	Potential ⁸
Sole Source Aquifer	No Detailed Groundwater Assessment	-	-	-	Detailed Groundwater Assessment
Floodplain	No Substantial Impacts	-	-	-	Substantial Impacts
Section 4(f) Impacts	None	-	-	-	Any ⁹
Section 6(f) Impacts	None	-	-	-	Any
Permanent Traffic Alteration	None	-	-	-	Any
Noise Analysis Required	No	-	-	-	Yes
Air Quality Analysis Required	No	-	-	-	Yes ¹⁰
Approval Level <ul style="list-style-type: none"> • District Env. (DE) • Env. Serv. Div. (ESD) • FHWA 	Concurrence by DE or ESD	DE or ESD	DE or ESD	DE and/or ESD	DE and/or ESD; and FHWA

¹ Coordinate with INDOT Environmental Services Division. INDOT will then coordinate with the appropriate FHWA Environmental Specialist.

² Any involvement with a bridge processed under the Historic Bridge Programmatic Agreement.

³ Total permanent impacts to streams (linear feet) and wetlands (acres).

⁴ US Army Corps of Engineers Individual 404 Permit

⁵ Total permanent and temporary right-of-way. This does not include reacquisition of existing apparent right-of-way.

⁶ Avoidance and Mitigation Measures (AMMs) determined by the IPAC determination key to be required that are not tree AMMs, bridge AMMs, or structure AMMs.

⁷ Projects that do not fall under a Species Specific Programmatic and results in a “Likely to Adversely Affect”. Other findings can be processed as a lower level CE.

⁸ Potential for causing a disproportionately high and adverse impact.

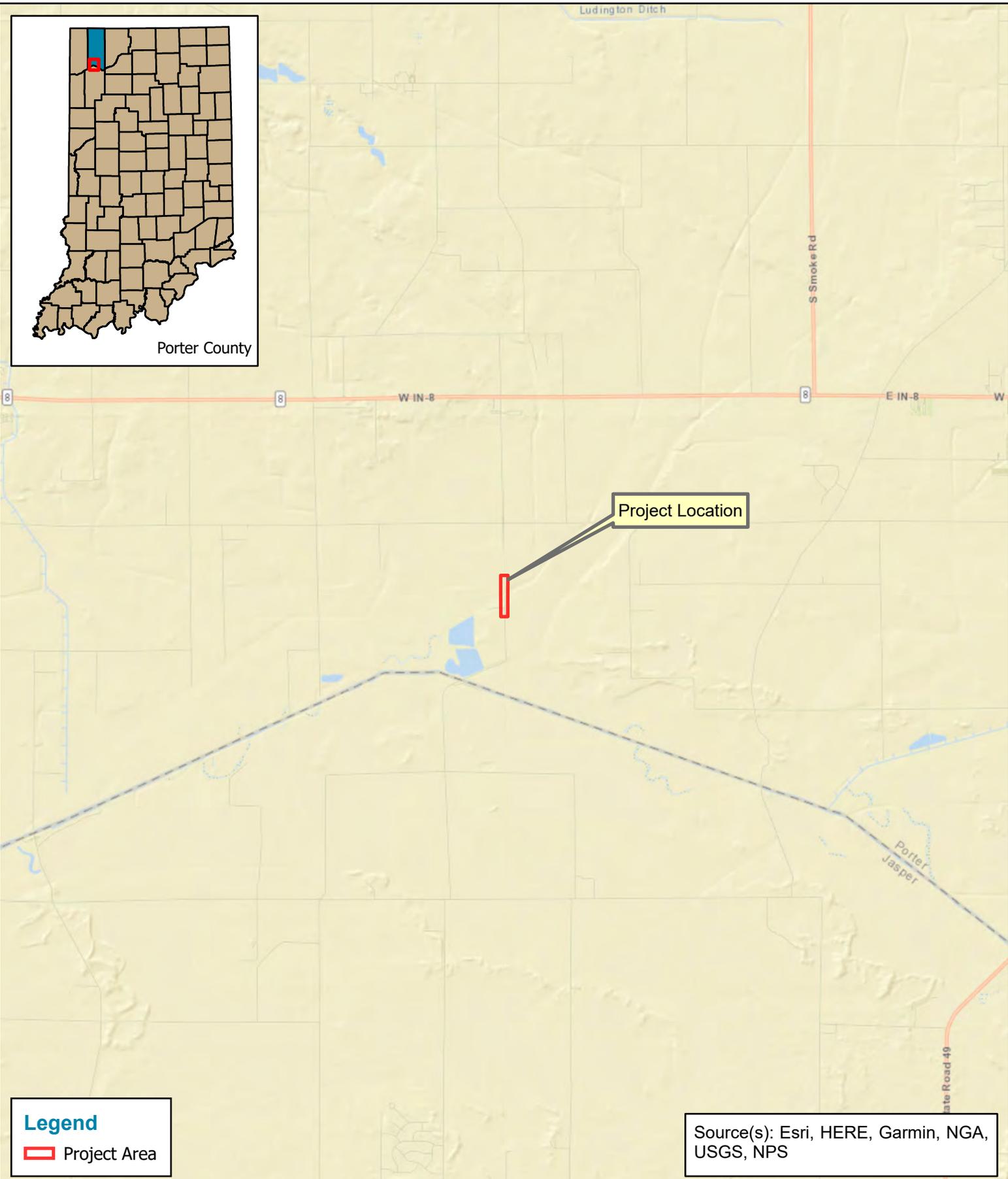
⁹ Section 4(f) use resulting in an Individual, Programmatic, or *de minimis* evaluation. The only exception is a *de minimis* evaluation for historic properties (Effective January 2, 2020). If a historic property *de minimis* and no other use, mark the *None* column.

¹⁰ Hot Spot Analysis and/or MSAT Quantitative Emission Analysis.

* Includes the threatened/endangered species critical habitat

Note: Substantial public or agency controversy may require a higher-level NEPA document.

Categorical Exclusion
Appendix B
Graphics



Legend

 Project Area

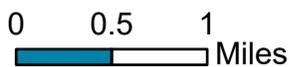
Source(s): Esri, HERE, Garmin, NGA, USGS, NPS



112 West Jefferson Blvd, Suite 500
 South Bend, IN 46601
 Phone: (574) 334-5460
 Toll Free: (800) 423-7411

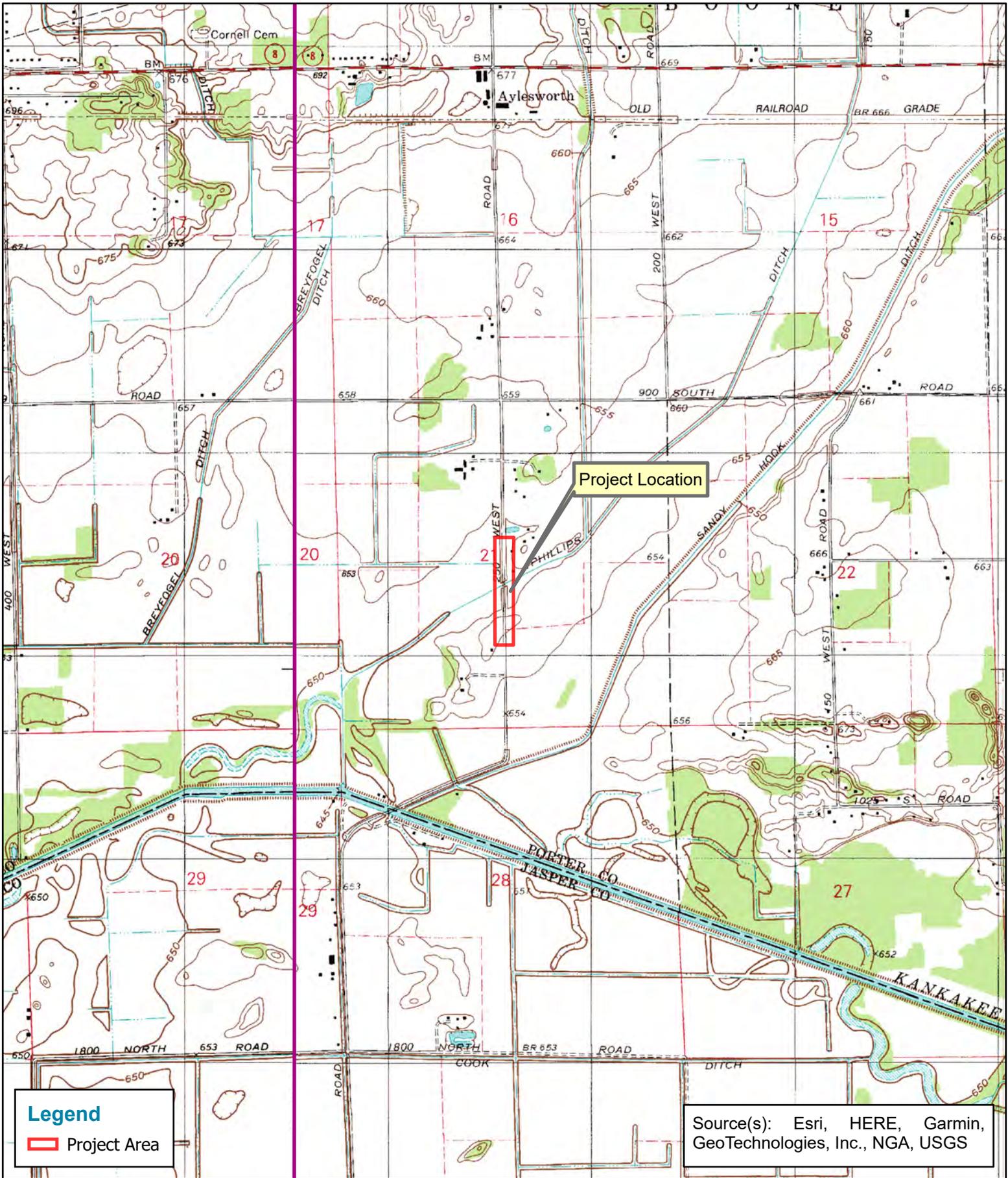
Location Map

Des. No. 2003041



County: Porter
 Township: Boone
 State: Indiana

CR 250 W over Phillips Ditch
 Bridge Replacement
 Created: 12/13/2022, CHoogewerf



Legend

 Project Area

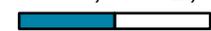
Source(s): Esri, HERE, Garmin, GeoTechnologies, Inc., NGA, USGS



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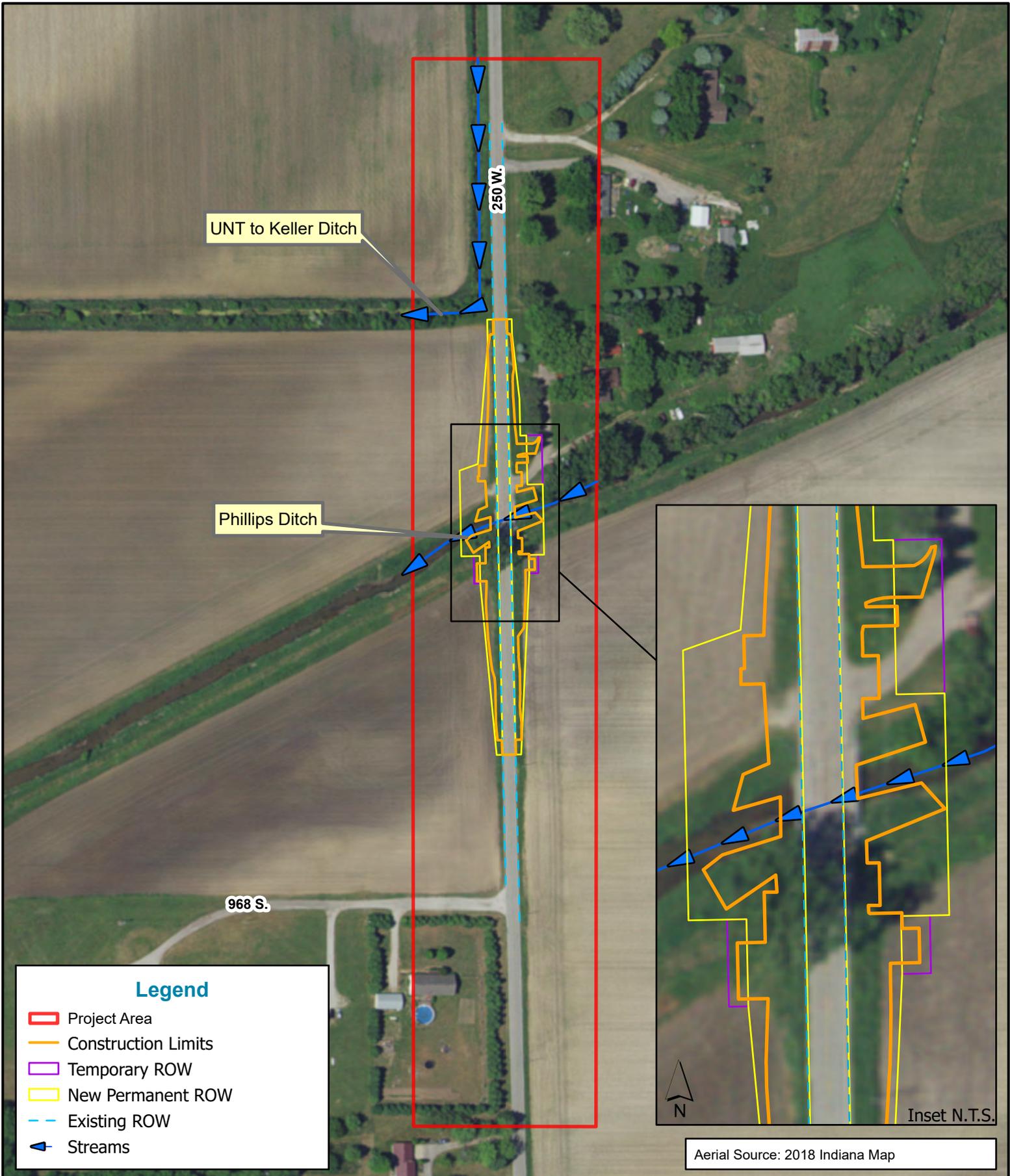
Topographic Map (1:24,000)
 USGS Quadrangle: KOUTS
 Des. No. 2003041

County: Porter
 Township: Boone
 State: Indiana

0 1,000 2,000
 Feet



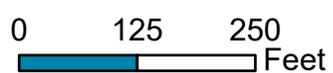
CR 250 W over Phillips Ditch
 Bridge Replacement
 Created: 12/13/2022, CHoogewerf



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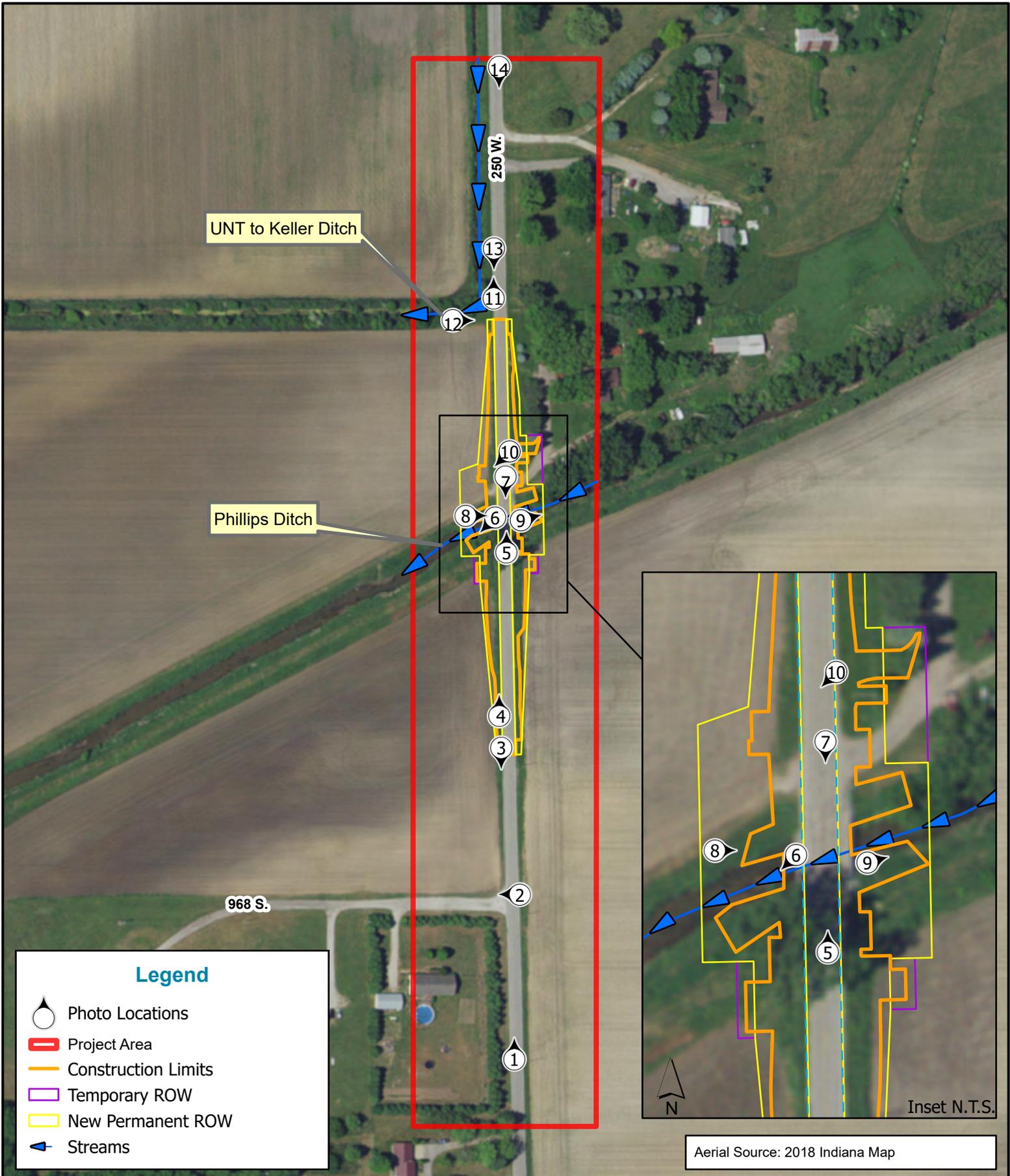
Aerial Map (2018)

Des. No. 2003041



County: Porter
 Township: Boone
 State: Indiana

CR 250 W over Phillips Ditch
 Bridge Project
 Created: 8/7/2023, CHoogewerf



Legend

- Photo Locations
- Project Area
- Construction Limits
- Temporary ROW
- New Permanent ROW
- Streams

Aerial Source: 2018 Indiana Map

LOCHMUELLER GROUP

112 West Jefferson Blvd, Suite 500
 South Bend, IN 46601
 Phone: (574) 334-5460
 Toll Free: (800) 423-7411

Photo Location Map
 Des. No. 2003041

0 125 250
 Feet

County: Porter
 Township: Boone
 State: Indiana

CR 250 W over Phillips Ditch
 Bridge Project
 Created: 8/7/2023, CHoogewerf



1. Looking north along CR 250 W from southern project terminus



2. Looking west toward CR 968 S



3. Looking south along west side of CR 250 W



4. Looking north along west side of CR 250 W



5. Looking north at bridge



6. Looking southwest along Phillips Ditch (downstream) from bridge



7. Looking south from west side of bridge



8. Looking east at bridge



9. Looking northeast along Phillips Ditch (upstream) from bridge



10. Looking southwest toward bridge



11. Looking north along west side of CR 250 W, UNT to Keller Ditch to left



12. Looking east along UNT to Keller Ditch



13. Looking south along west side of CR 250 W, UNT to Keller Ditch to right



14. Looking south along CR 250 W from northern project terminus

PROJECT	DESIGNATION
2003041	2003041
CONTRACT	BRIDGE FILE
B-44107	64-00066 B

STRUCTURE INFORMATION				
STRUCTURE	TYPE	SPAN AND SKEW	OVER	STATION
64-00066 B	Composite Prestressed Concrete Box Beam	1 Span @ 61'-0" Skew: 25° Lt.	Phillips Ditch	56+51.57 Line "A"

INDIANA DEPARTMENT OF TRANSPORTATION



BRIDGE PLANS FOR SPANS OVER 20 FEET

ROUTE: IR 7702 AT: PHILLIPS DITCH
PROJECT NO. 2003041 P.E.
2003041 R/W
2003041 CONST.

ADDITIONAL RIGHT-OF-WAY
REQUIRED FOR THIS PROJECT

PFC Plans
December 12, 2022

APPROVED BY:
PORTER COUNTY BOARD OF COMMISSIONERS

Jeff Good, President DATE

Laura Shurr Blaney, Vice-President DATE

Jim Biggs, Secretary DATE

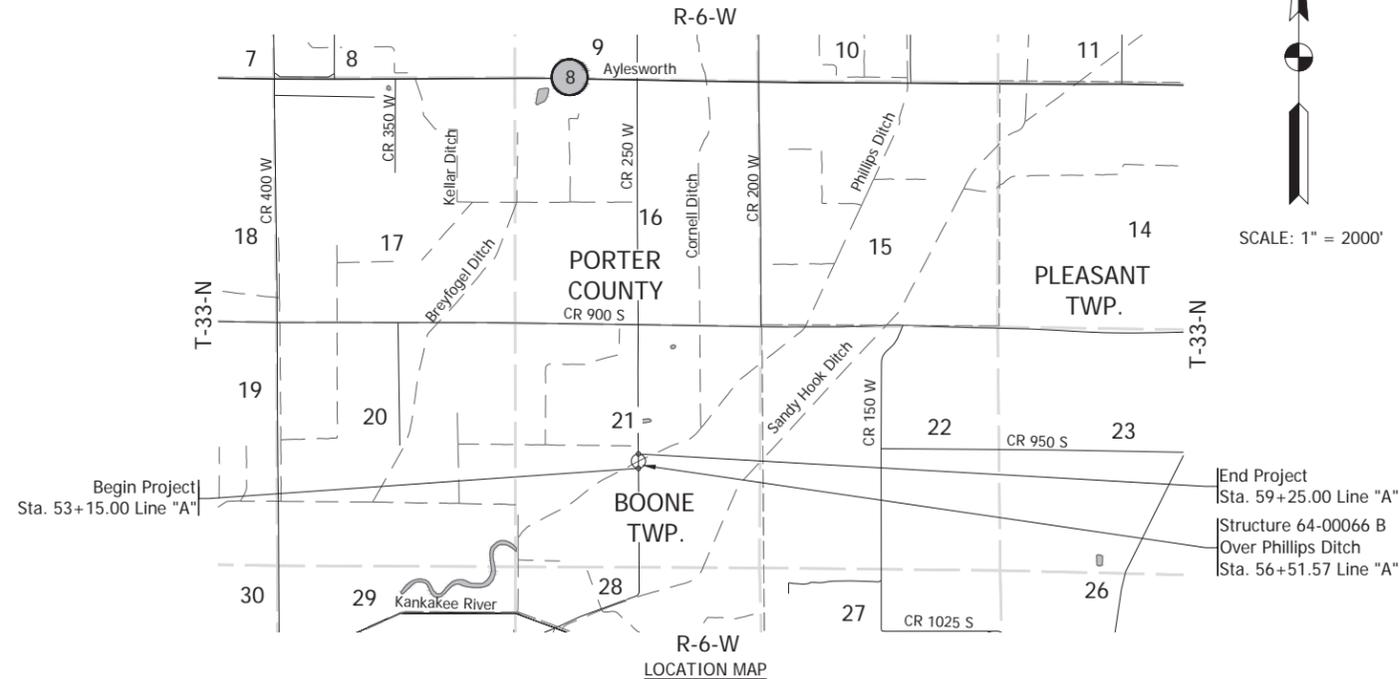
ATTEST BY:
PORTOR COUNTY AUDITOR

Vicki Urbanik, County Auditor DATE

RECOMMENDED FOR APPROVAL BY:
PORTER COUNTY DEPARTMENT OF DEVELOPMENT
AND STORM WATER MANAGEMENT

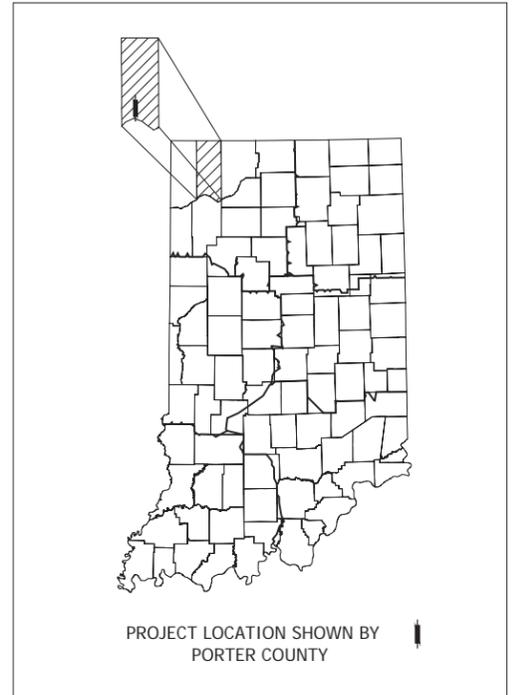
Robert Thompson, AICP, Director and Employee in Responsible Charge DATE

Bridge Replacement on IR 7702, County Road 250 West over Phillips Ditch
Located Approximately 1.50 Miles South of SR 8 in
Section 21, T-33-N, R-6-W, Boone Township, Porter County



TRAFFIC DATA		
A.A.D.T. (2026)		868 V.P.D.
A.A.D.T. (2046)		1059 V.P.D.
D.H.V. (2046)		111 V.P.H.
DIRECTIONAL DISTRIBUTION		50%
TRUCKS		18.1% A.A.D.T. 11.1% D.H.V.

DESIGN DATA	
DESIGN SPEED	45 M.P.H.
PROJECT DESIGN CRITERIA	3R (Non-Freeway)
FUNCTIONAL CLASSIFICATION	RURAL MAJOR COLLECTOR
RURAL/URBAN	RURAL (SUBURBAN)
TERRAIN	LEVEL
ACCESS CONTROL	NONE



LATITUDE: 41° 17' 43" LONGITUDE: 87° 06' 46"

BRIDGE LENGTH:	0.012	MI.
ROADWAY LENGTH:	0.104	MI.
TOTAL LENGTH:	0.116	MI.
MAX. GRADE:	2.23	%

HUC 12: 071200011009

PLANS PREPARED BY:	LOCHMUELLER GROUP, INC.	(574) 334-5460 PHONE NUMBER
CERTIFIED BY:	COVERING BRIDGE DESIGN, REGISTERED PROFESSIONAL ENGINEER STATE OF INDIANA NO.	-- / -- / --
CERTIFIED BY:	COVERING ROADWAY DESIGN, REGISTERED PROFESSIONAL ENGINEER STATE OF INDIANA NO.	-- / -- / --
APPROVED FOR LETTING:	INDIANA DEPARTMENT OF TRANSPORTATION	-- / -- / --

INDIANA DEPARTMENT OF TRANSPORTATION
STANDARD SPECIFICATIONS DATED 2022
TO BE USED WITH THESE PLANS

BRIDGE FILE	
64-00066 B	
DESIGNATION	
2003041	
SURVEY BOOK	SHEETS
ELECTRONIC	1 of 24
CONTRACT	PROJECT
B-44107	2003041



Date: Sep 07, 2023, 1:04pm User Name: NGeer File: X:\Production\Files\2021\21-303\PA-D-01\CAD\Plans\Title Sheet Porter No.66.dwg

UTILITIES

TELEPHONE:

NORTHWESTERN INDIANA TELEPHONE COMPANY
 DON SCHOENBECK
 PO BOX 67
 HEBRON, IN 46341
 219-996-0216

ELECTRIC:

KANKAKEE VALLEY R.E.M.C.
 JACOB BAILEY
 P.O. BOX 157
 WANATAH, IN 46390
 (219) 733-2511
 WORKORDERS@KVREMC.COM



Know what's below.
 Call before you dig.

NOTE TO REVIEWER:

UTILITY INFORMATION WILL BE UPDATED UPON
 COMPLETION OF UTILITY COORDINATION.

INDEX

SHEET NO.	SUBJECT
1	TITLE SHEET
2	INDEX
3	TYPICAL CROSS SECTIONS
4	PLAT NO. 1
5	MAINTENANCE OF TRAFFIC
6 - 7	PLAN AND PROFILE
8 - 9	LAYOUT
10 - 11	GENERAL PLAN
12	BRIDGE SUMMARY
13	ROAD SUMMARY
14 - 24	CROSS SECTIONS

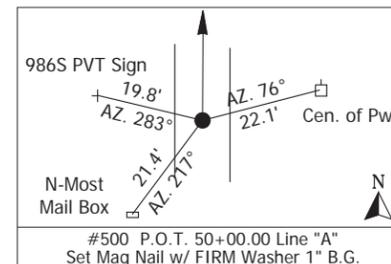
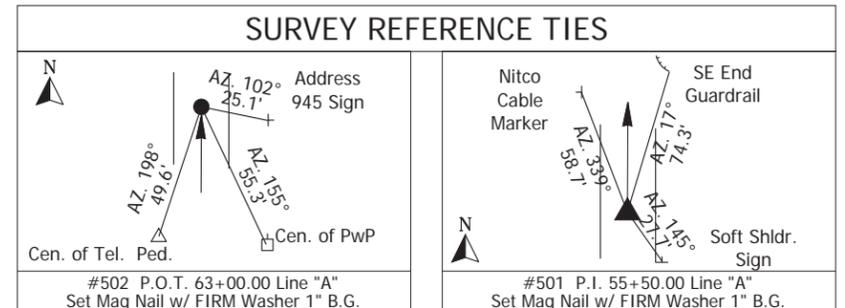
GENERAL NOTES

--

REVISIONS

SHEET NO.	DATE	REVISED

SURVEY REFERENCE TIES



COORDINATE LISTING: RANDOM CONTROL POINTS

PT. #	NORTHING	EASTING	DESCRIPTION
1400	335024.900	783650.131	SET MAG WITH REF WASHER RECESSED
1401	335098.301	783875.988	SET 3/4" REBAR W/ REF CAP FLUSH
6407	335683.795	783928.339	FND. REBAR W/ DECKARD CAP #20400021 FLUSH (SHOWN AS PROP. COR.)

COORDINATE LISTING: PROPERTY AND SECTION CORNER EVIDENCE FOUND

PT. #	NORTHING	EASTING	DESCRIPTION
6400	338073.702	783881.720	FND. HARRISON MARKER FLUSH
6401	336746.894	783902.722	FND. 1" OD I.P.I.PE 15" UP
6402	333556.071	783928.831	FND. REBAR W/ BENGAL CAP #12226 FLUSH (NOT SHOWN)
6403	333656.002	783928.133	FND. PINCH TOP PIPE 2" BG (NOT SHOWN)
6404	333102.453	783938.811	FND. PINCH TOP PIPE FLUSH
6405	335420.726	783928.216	FND. 1.5" OD I.P.I.PE 6" BG
6406	335423.764	783933.330	FND. REBAR W/ DECKARD CAP #20400021 FLUSH
6407	335683.795	783928.339	FND. REBAR W/ DECKARD CAP #20400021 FLUSH
6408	336083.832	783920.440	FND. 1.5" OD I.P.I.PE FLUSH

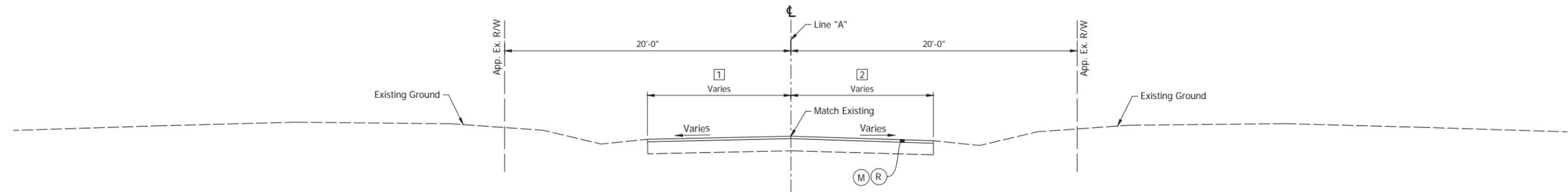
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DESIGNED: _____ NB _____	DRAWN: _____ NCG _____	
CHECKED: _____ EJJ _____	CHECKED: _____ NB _____	

INDIANA
 DEPARTMENT OF TRANSPORTATION

INDEX

HORIZONTAL SCALE	BRIDGE FILE
N/A	64-00066 B
VERTICAL SCALE	DESIGNATION
N/A	2003041
SURVEY BOOK	SHEETS
ELECTRONIC	2 of 24
CONTRACT	PROJECT
B-44107	2003041

Date: Sep 07, 2023, 1:04pm User Name: NGreer File: X:\Production\Files\2021\121-3027\PRJ-01\CAD\Plans\Index\Porter No. 66.dwg

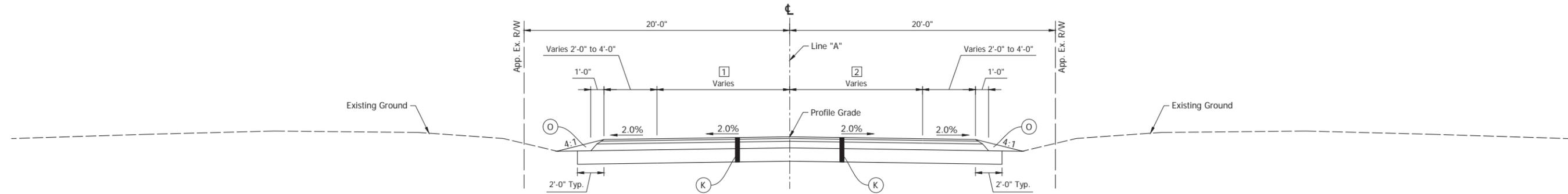


9.8' at Sta. 52+65.00 to Sta. 52+90.00 [1]
 9.4' at Sta. 59+50.00 to Sta. 59+75.00

TYPICAL INCIDENTAL SECTION

Sta. 52+65.00 Line "A" To Sta. 52+90.00 Line "A"
 Sta. 59+50.00 Line "A" To Sta. 59+75.00 Line "A"

[2] 10.2' at Sta. 52+65.00 to Sta. 52+90.00
 10.9' at Sta. 59+50.00 to Sta. 59+75.00

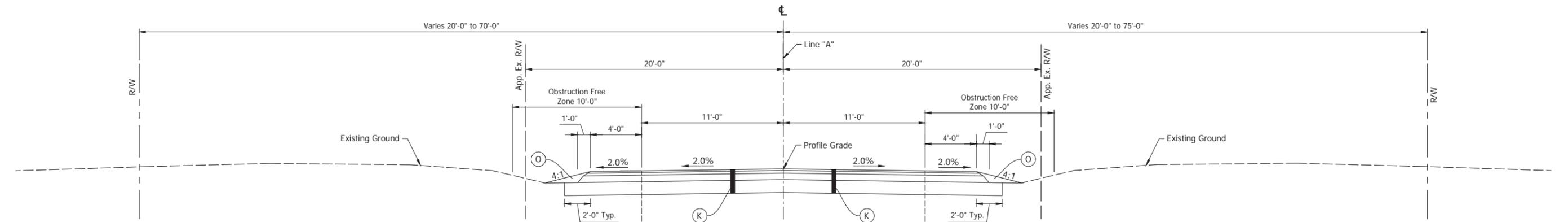


Varies from 9.8' at Sta. 52+90.00 to 11.0' at Sta. 53+15.00 [1]
 Varies from 11.0' at Sta. 59+25.00 to 9.4' at Sta. 59+50.00

TYPICAL FULL DEPTH INCIDENTAL SECTION

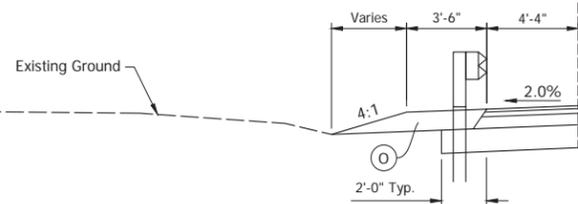
Sta. 52+90.00 Line "A" To Sta. 53+15.00 Line "A"
 Sta. 59+25.00 Line "A" To Sta. 59+50.00 Line "A"

[2] Varies from 10.2' at Sta. 52+90.00 to 11.0' at Sta. 53+15.00
 Varies from 11.0' at Sta. 59+25.00 to 10.9' at Sta. 59+50.00

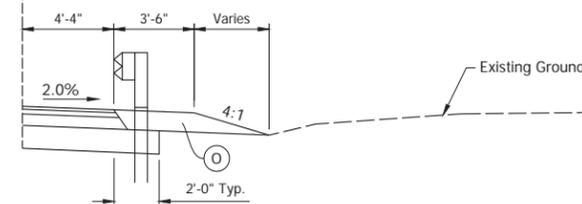


TYPICAL FULL DEPTH SECTION

Sta. 53+15.00 Line "A" To Sta. 55+92.19 Line "A"
 Sta. 57+10.94 Line "A" To Sta. 59+25.00 Line "A"



Sta. 55+57.52 to Sta. 55+97.27 Line "A"
 Sta. 56+91.89 to Sta. 57+31.64 Line "A"



Sta. 55+71.51 to Sta. 56+11.26 Line "A"
 Sta. 57+05.88 to Sta. 57+45.63 Line "A"

- (K) 165 LB/SYS QC/OA HMA, 3, 64, Surface 9.5mm on 275 LB/SYS QC/OA HMA, 3, 64, Intermediate 19.0mm on 660 LB/SYS QC/OA HMA, 3, 64, Base 25.0mm on Subgrade Treatment, Type IC on Geotextile for Pavement Type 2B
- (M) Milling, Asphalt, 1 1/2 IN.

- (R) 165 LB/SYS QC/OA HMA, 3, 64, Surface 9.5 mm
- (O) Compacted Aggregate No. 53

RECOMMENDED FOR APPROVAL _____ DESIGN ENGINEER _____ DATE _____

DESIGNED: JAG DRAWN: JAG

CHECKED: BSS CHECKED: BSS

INDIANA DEPARTMENT OF TRANSPORTATION

TYPICAL CROSS SECTIONS

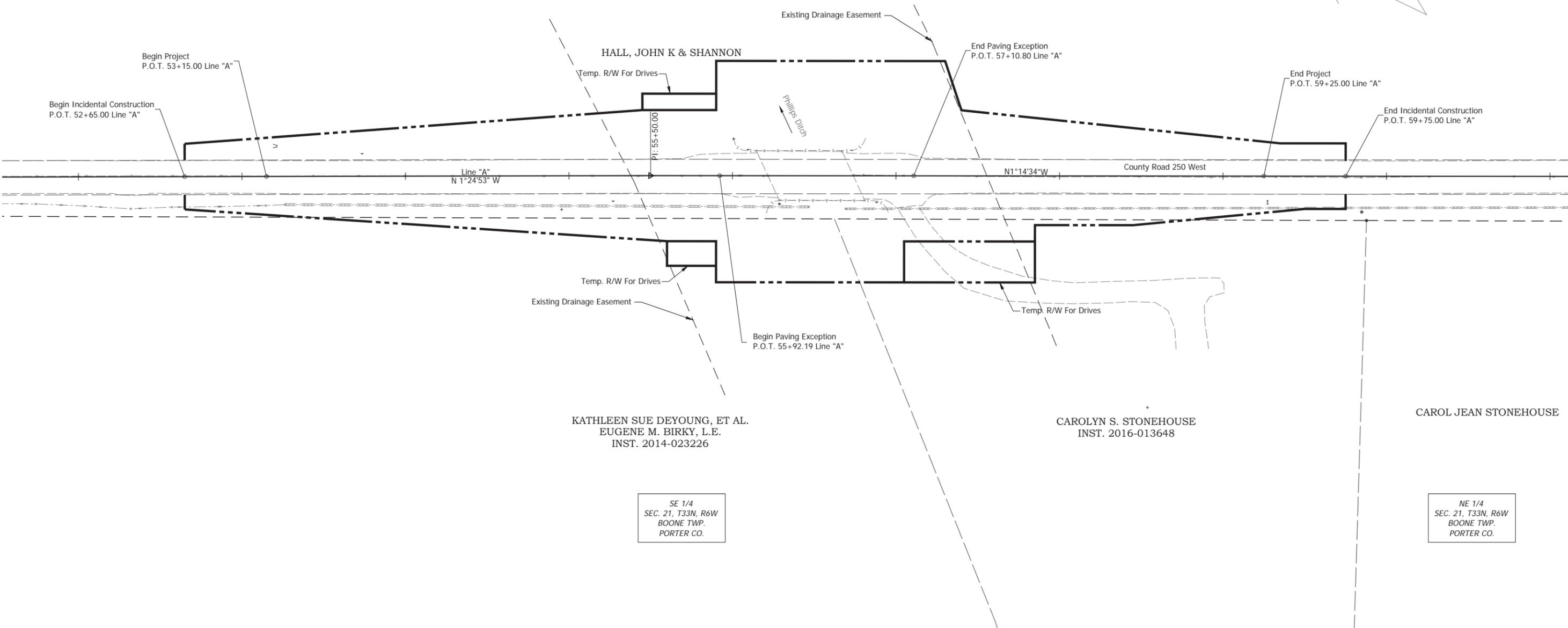
HORIZONTAL SCALE	BRIDGE FILE
1/4" = 1'-0"	64-00066 B
VERTICAL SCALE	DESIGNATION
N/A	2003041
SURVEY BOOK	SHEET
ELECTRONIC	3 of 24
CONTRACT	PROJECT
B-44107	2003041



SW 1/4
SEC. 21, T33N, R6W
BOONE TWP.
PORTER CO.

NW 1/4
SEC. 21, T33N, R6W
BOONE TWP.
PORTER CO.

52+00 53+00 54+00 55+00 56+00 57+00 58+00 59+00 60+00 61+00



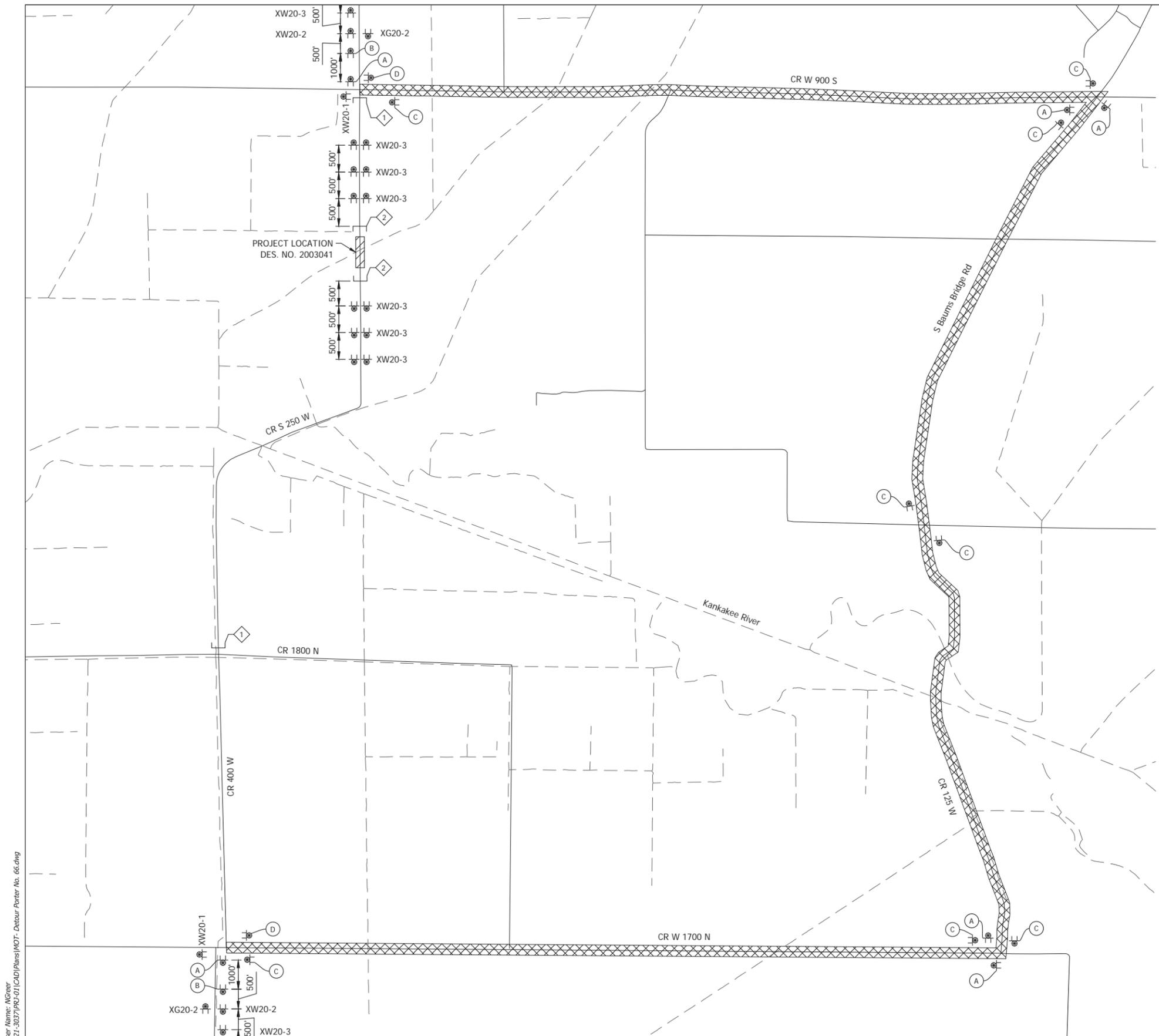
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RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: JAG	DRAWN: JAG	
CHECKED: BSS	CHECKED: BSS	

INDIANA
DEPARTMENT OF TRANSPORTATION

PLAT NO. 1

HORIZONTAL SCALE	BRIDGE FILE
1" = 30'	64-00066 B
VERTICAL SCALE	DESIGNATION
N/A	2003041
SURVEY BOOK	SHEET
ELECTRONIC	4 of 24
CONTRACT	PROJECT
B-44107	2003041

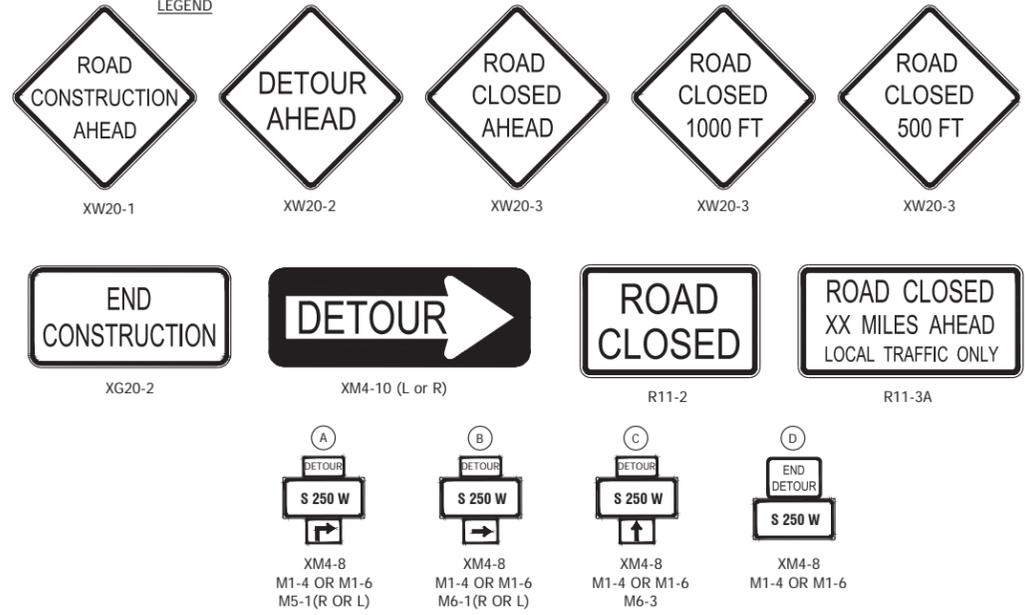


CONSTRUCTION SIGN SCHEDULE					
	DESCRIPTION	SIZE (IN)	TYPE	QUANTITY	
R11-2	"ROAD CLOSED" SIGN	48 x 30	(1)	2	
R11-3A	"ROAD CLOSED XX MILES AHEAD" SIGN	60 x 30	(1)	2	
XM4-10 (L or R)	"DETOUR" SIGN	48 x 18	(1)	2	
XW20-1	"ROAD CONSTRUCTION AHEAD" SIGN	48 x 48	A	2	
XW20-2	"DETOUR AHEAD" SIGN	48 x 48	A	2	
XW20-3	"ROAD CLOSED AHEAD" SIGN	48 x 48	A	14	
XG20-2	"END CONSTRUCTION" SIGN	36 x 18	B	2	
				TOTAL TYPE "A" SIGNS	18
				TOTAL TYPE "B" SIGNS	2

Detour Route Marker Assemblies: 18 Req'd
 Road Closure Sign Assemblies: 4 Req'd
 Type III-A Barricades: 48 Lft.
 Type III-B Barricades: 24 Lft.

(1) Included with road closure sign assembly.

LEGEND



- 1 Road Closure Sign Assembly w/ Type III-B Barricade (12 Lft.) and R11-3A and XM4-10 (L or R)
- 2 Road Closure Sign Assembly w/ Type III-A Barricade (24 Lft.) and R11-2

Construction Zone
 Detour Route

GENERAL NOTES

1. All maintenance of traffic devices, signs and pavement markings shall conform to the latest edition of the Indiana MUTCD.
2. See INDOT Std. Drwg. 801-TCDDT-01 for sign spacing requirements and additional notes.
3. See INDOT Std. Drwg. 801-TCLG-01 for standard notes.
4. Type B construction warning lights shall be used with all signs located on barricades. Type A construction warning lights shall be used on all other construction signs.

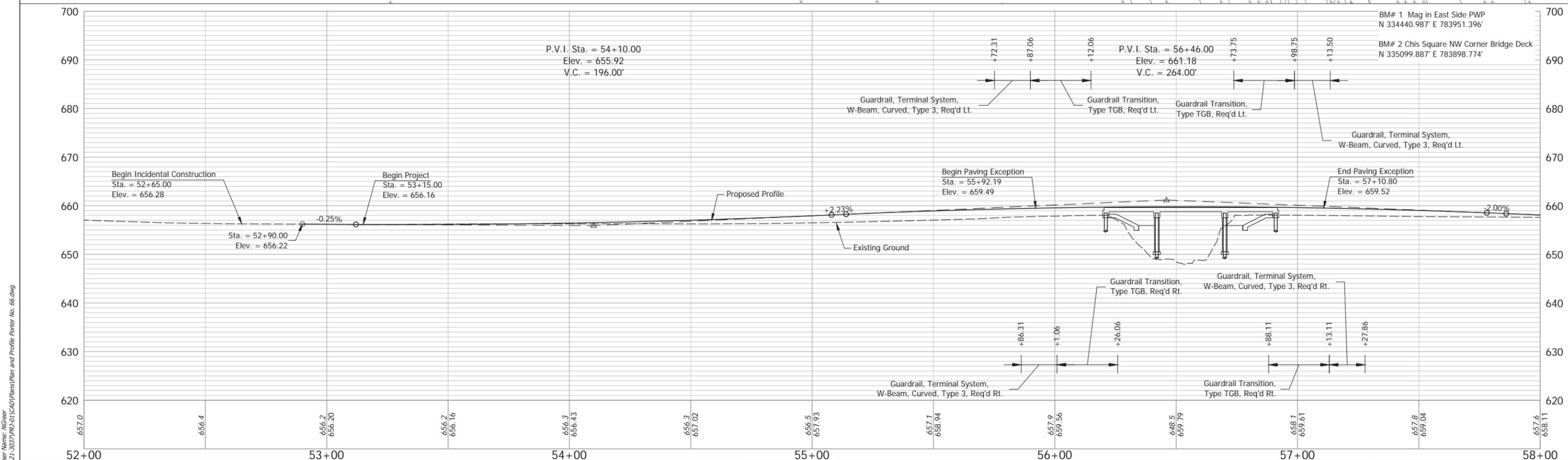
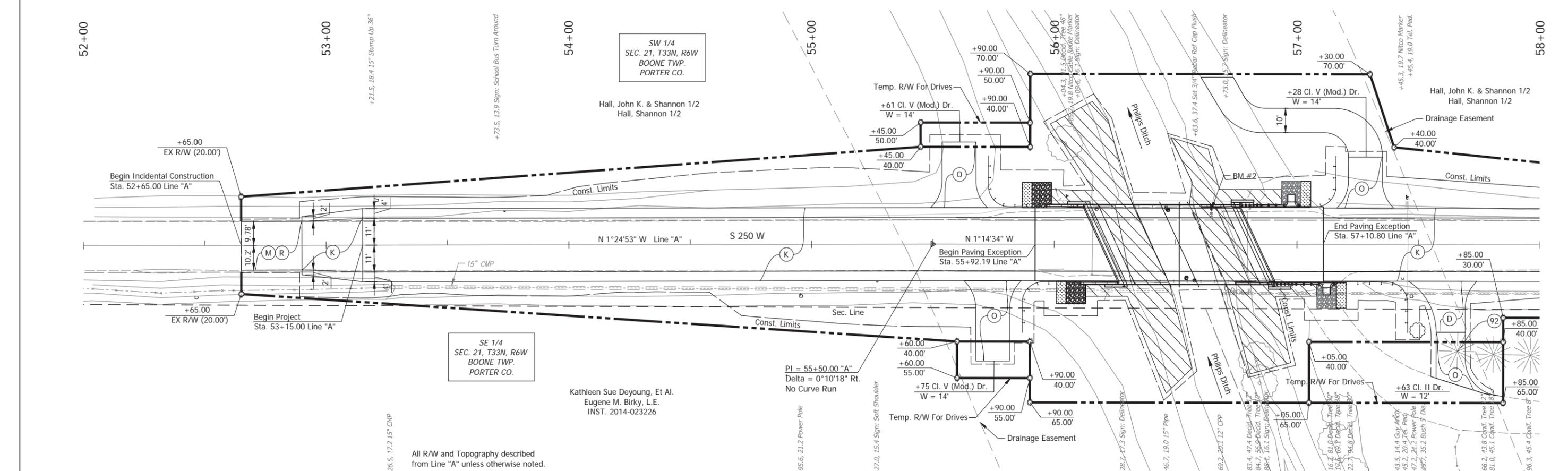
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RECOMMENDED FOR APPROVAL _____	DESIGN ENGINEER _____	DATE _____
DESIGNED: _____ DJM	DRAWN: _____ DJM	
CHECKED: _____ BSS	CHECKED: _____ BSS	

INDIANA
 DEPARTMENT OF TRANSPORTATION

 MAINTENANCE OF TRAFFIC
 DETOUR

HORIZONTAL SCALE	BRIDGE FILE
1" = 1000'	64-00066 B
VERTICAL SCALE	DESIGNATION
N/A	2003041
SURVEY BOOK	SHEET
ELECTRONIC	5 of 24
CONTRACT	PROJECT
B-44107	2003041



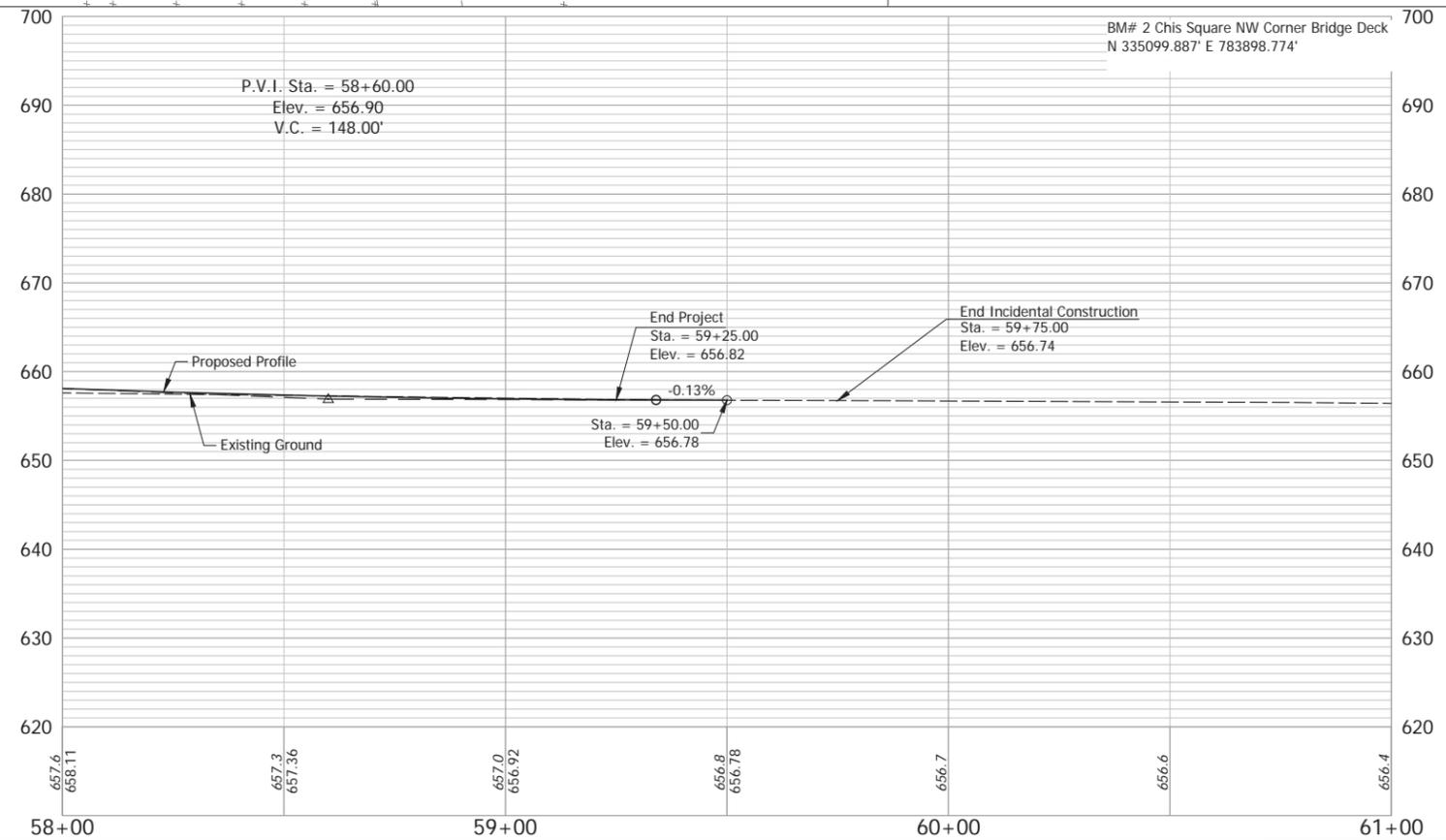
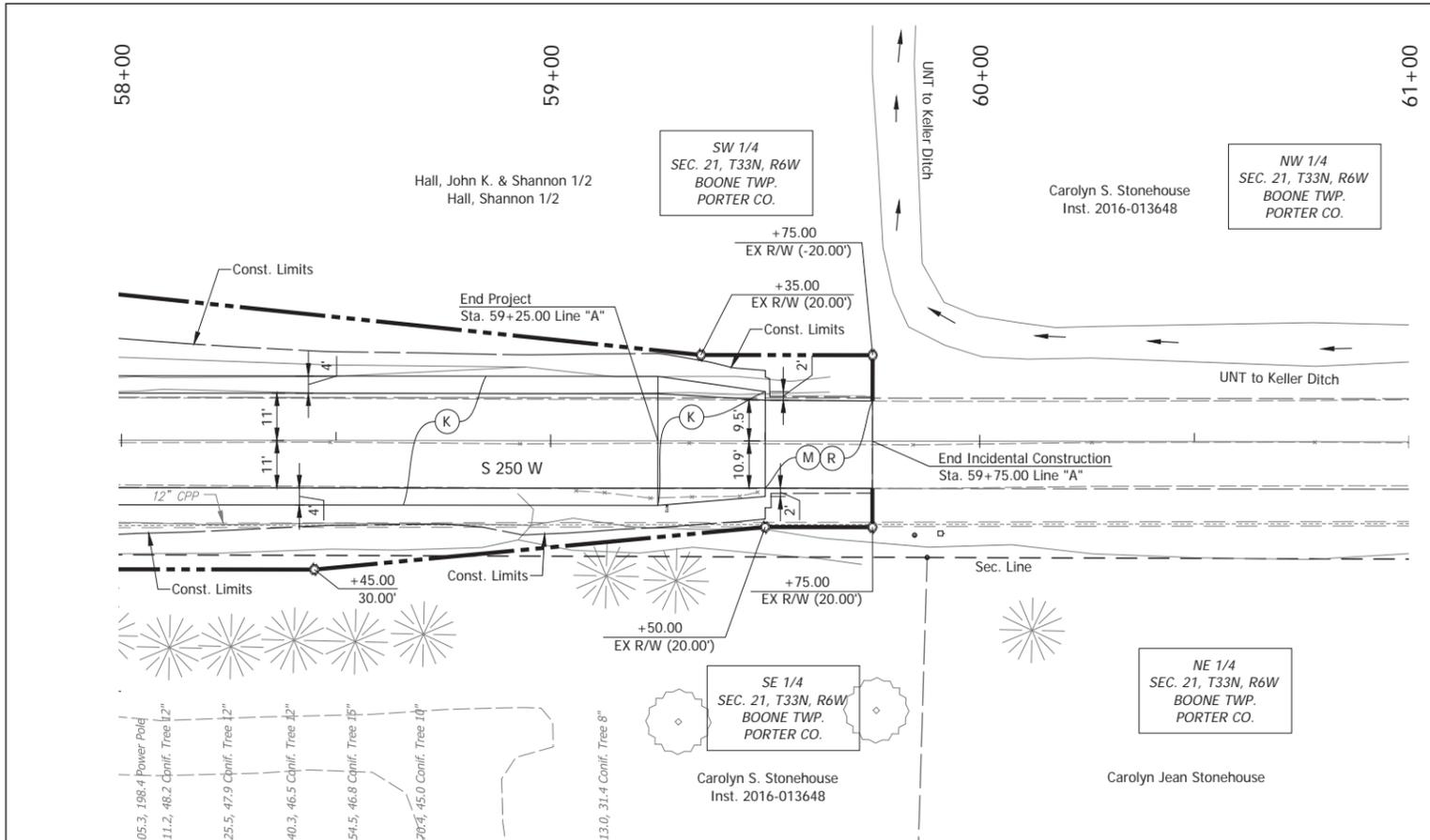
(D)	HMA for Approaches, Type B 165 lbs/syd HMA Surface Type B on 385 lbs/syd HMA Intermediate Type B on Subgrade Treatment Type II	(K)	165 LB/SYS QC/QA HMA, 3, 64, Surface 9.5mm on 275 LB/SYS QC/QA HMA, 3, 64, Intermediate 19.0mm on 660 LB/SYS QC/QA HMA, 3, 64, Base 25.0mm on Subgrade Treatment, Type IC on Geotextile for Pavement Type 2B	(R)	165 LB/SYS QC/QA HMA, 3, 64, Surface 9.5 mm
(M)	Milling, Asphalt, 1 1/2 IN.	(92)	Tree, Remove		
(O)	Compacted Aggregate No. 53				

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: MMT	DRAWN: MMT	
CHECKED: BSS	CHECKED: BSS	

INDIANA DEPARTMENT OF TRANSPORTATION

PLAN AND PROFILE

HORIZONTAL SCALE	BRIDGE FILE
1" = 20'	64-00066 B
VERTICAL SCALE	DESIGNATION
1" = 10'	2003041
SURVEY BOOK	SHEET
ELECTRONIC	6 of 24
CONTRACT	PROJECT
B-44107	2003041



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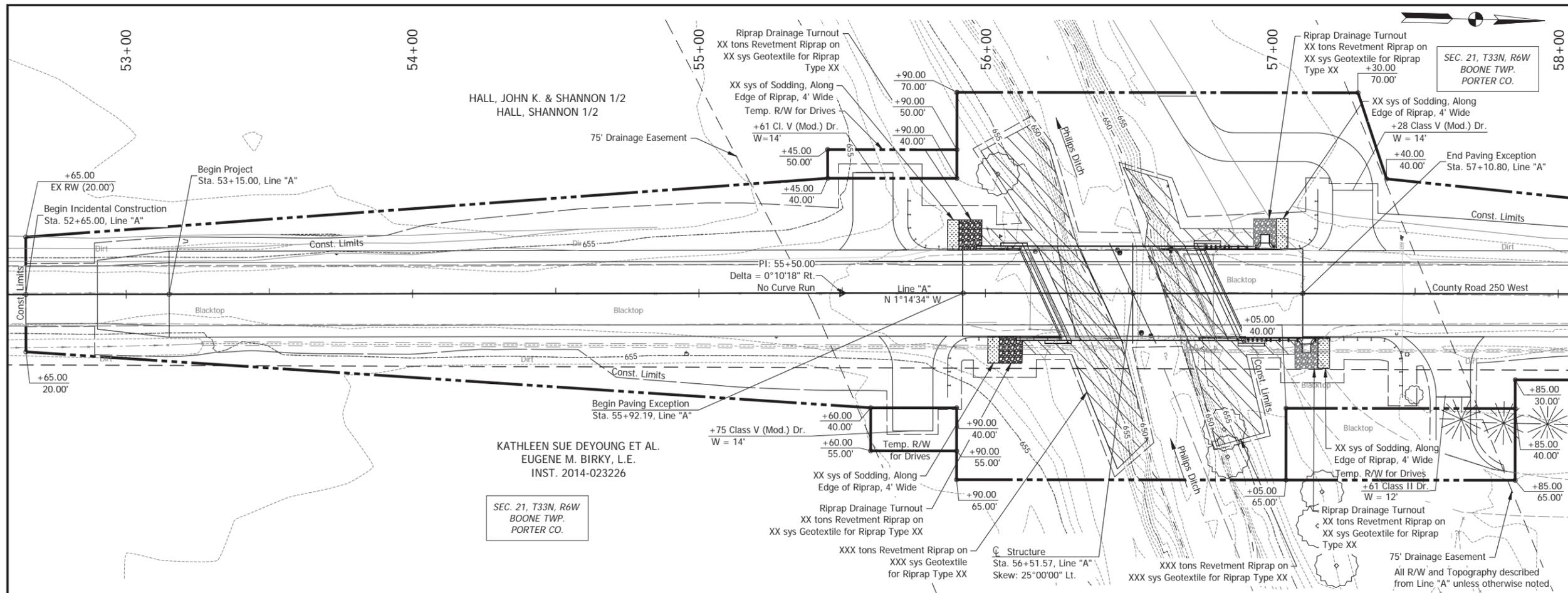
- (K) 165 LB/SYS QC/QA HMA, 3, 64, Surface 9.5mm on
275 LB/SYS QC/QA HMA, 3, 64, Intermediate 19.0mm on
660 LB/SYS QC/QA HMA, 3, 64, Base 25.0mm on
Subgrade Treatment, Type IC on Geotextile for Pavement Type 2B
- (M) Milling, Asphalt, 1 1/2 IN.
- (R) 165 LB/SYS QC/QA HMA, 3, 64, Surface 9.5 mm

RECOMMENDED FOR APPROVAL _____	DESIGN ENGINEER _____ DATE _____
DESIGNED: _____ MMT _____	DRAWN: _____ MMT _____
CHECKED: _____ BSS _____	CHECKED: _____ BSS _____

INDIANA
DEPARTMENT OF TRANSPORTATION

PLAN AND PROFILE

HORIZONTAL SCALE 1" = 20'	BRIDGE FILE 64-00066 B
VERTICAL SCALE 1" = 10'	DESIGNATION 2003041
SURVEY BOOK	SHEET
ELECTRONIC	7 of 24
CONTRACT B-44107	PROJECT 2003041



EXISTING STRUCTURE

The existing structure, 64-00066, is a single span prestressed concrete adjacent box beam bridge built in 1930 and reconstructed in 1996, with a 50.0' span and 30.0' clear roadway. Existing structure to be removed.

HYDRAULIC DATA

Drainage Area	19.05 SQ. MI.
Q100 Discharge	1,190 CFT./SEC.
Q100 Elevation	655.50 M.S.L.
Q100 Backwater	0.81 FT.
Q100 Velocity	5.55 FT./SEC.
Proposed Waterway Opening, Below Q100	228.16 SFT.
Low Structure Elevation	656.34 M.S.L.
Skew	25°00'00"
Existing Waterway Opening	200.26 SFT.
Existing Low Structure Elevation	656.08 M.S.L.
Existing Backwater	1.02 FT.

HYDRAULIC SCOUR DATA

Q100 Discharge	1,190 CFT./SEC.
Q100 Elevation	655.50 M.S.L.
Q100 Scour Velocity	6.74 FT./SEC.
Q100 Contraction Scour Depth	3.51 FT.
Q100 Total Scour Depth	3.51 FT.
Q100 Low Scour Elevation	643.41 M.S.L.
Q500 Discharge	1,547 CFT./SEC.
Q500 Elevation	655.91 M.S.L.
Q500 Scour Velocity	8.06 FT./SEC.
Q500 Contraction Scour Depth	10.82 FT.
Q500 Total Scour Depth	10.82 FT.
Q500 Low Scour Elevation	636.10 M.S.L.

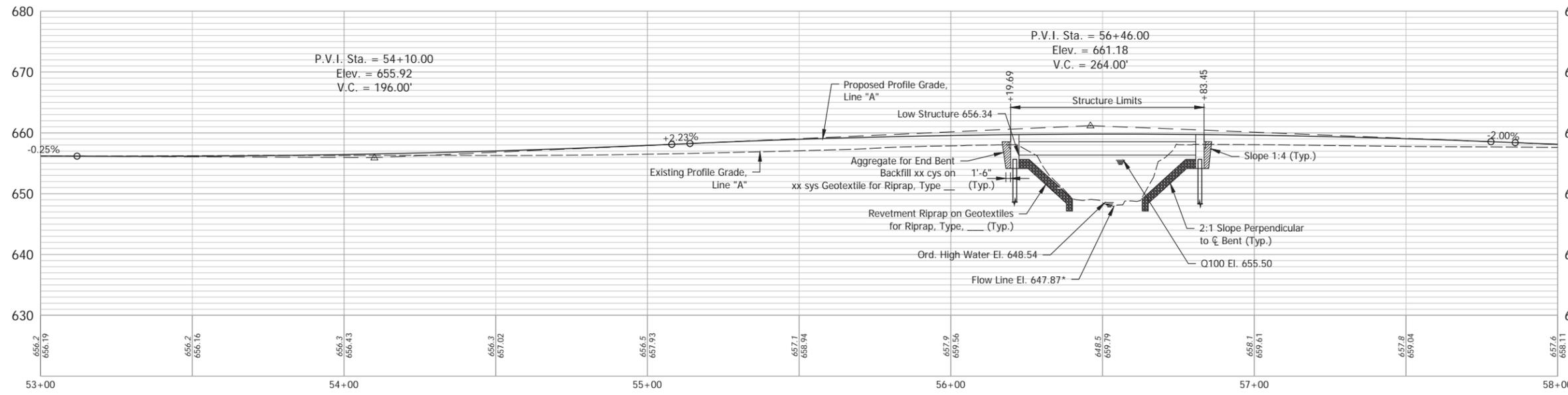
*Flowline elevation shown is taken at the centerline alignment, Line "A". This elevation differs from 646.92 from the hydraulic calculations. The hydraulic calculations show the lowest elevation from the HEC-RAS model (which is downstream of the downstream face) to conservatively check for scour.

EARTHWORK TABULATION

Fill	xxxx cys
Fill + 20%	xxxx cys
Common Excavation	xxxx cys
Usable Waterway Excavation	x cys
Borrow	xxxx cys
Total Waterway Excavation	x cys
Excavation Foundation Unclassified	xxx cys
Benching (Estimated)	xxxx cys

No direct payment for Benching. Benching will not be paid for as Common Excavation.

- NOTES**
- See Sheet 2 for utility owners and survey reference ties.
 - See Plan and Profile Sheet for approach work, incidental construction, Bench Mark, and additional details.
 - M.S.L. = Mean Sea Level.



**COMPOSITE PRESTRESSED CONCRETE
BOX BEAM BRIDGE**
1 SPAN: 61'-0"
30'-0" CLEAR ROADWAY; 25° SKEW LT.
COUNTY ROAD 250 WEST OVER PHILLIPS DITCH
PORTER COUNTY

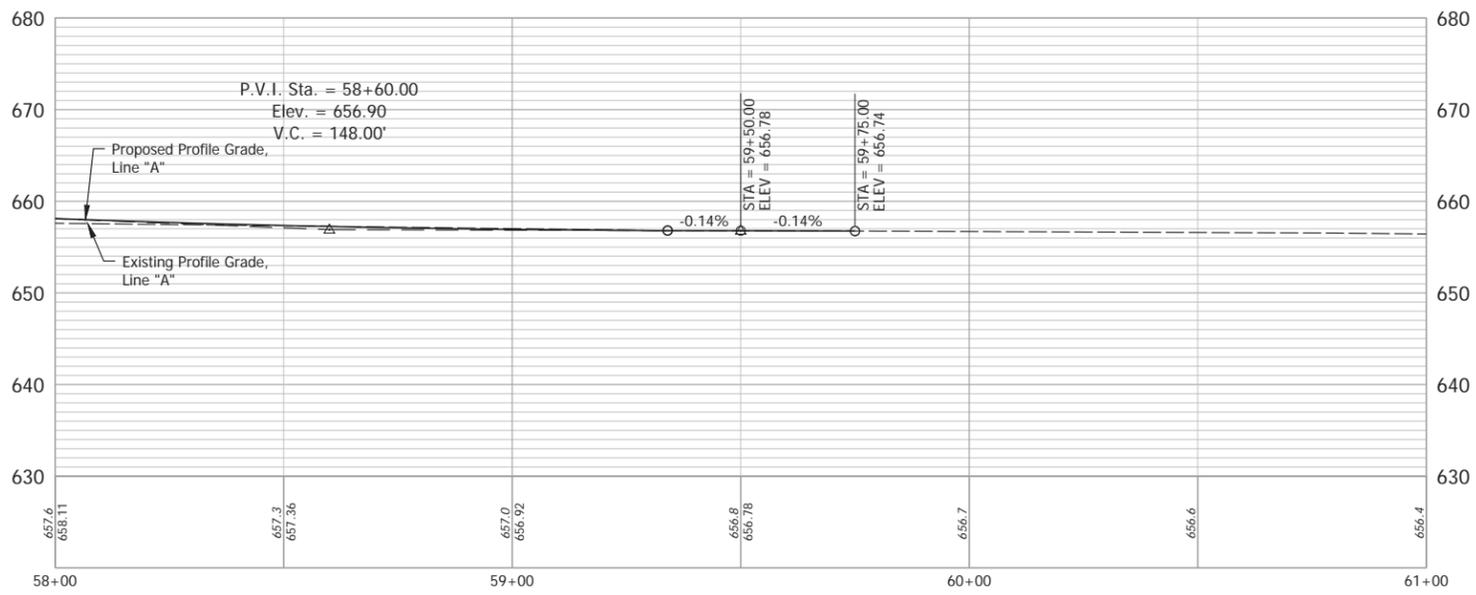
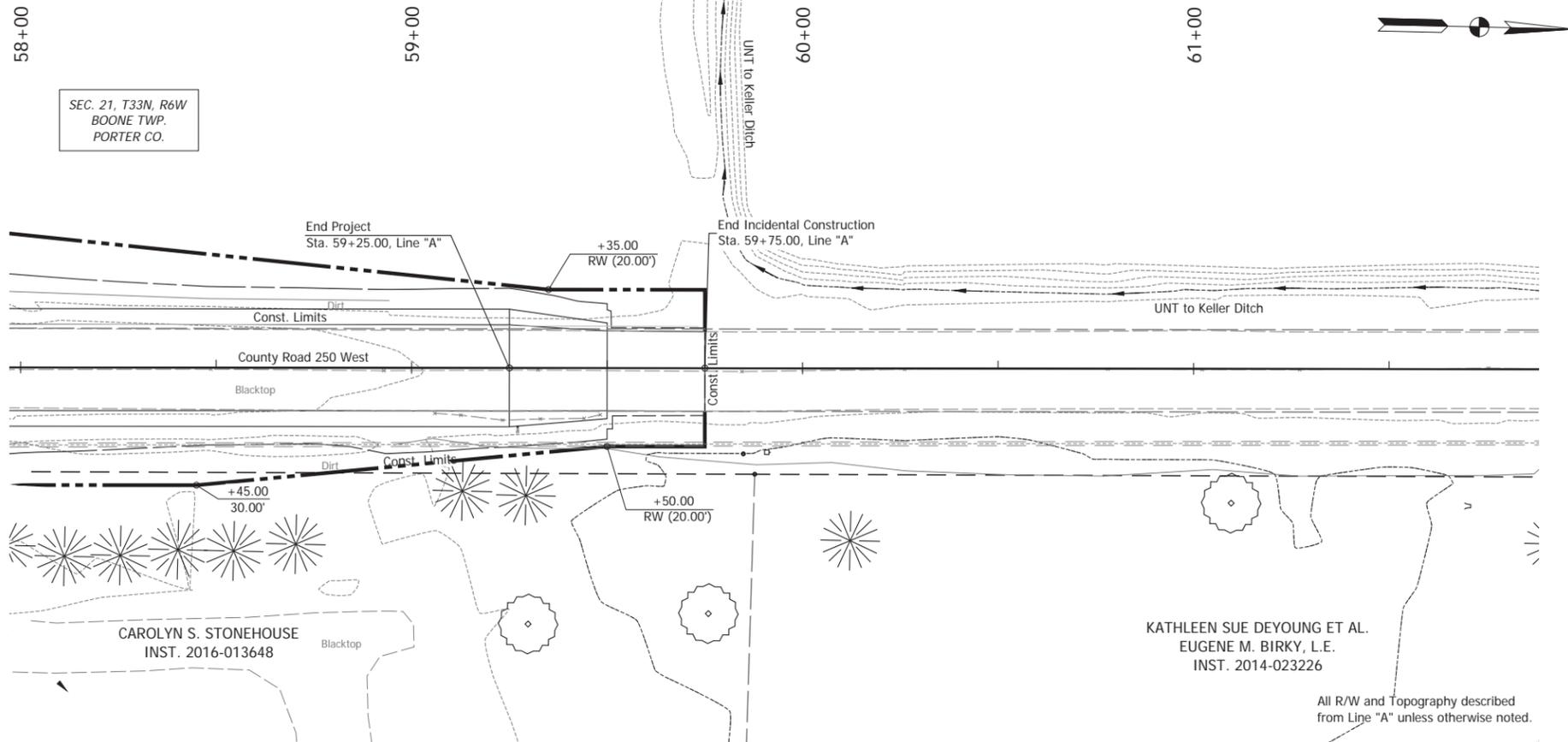
RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: NB	DRAWN: NCG	
CHECKED: EJG	CHECKED: NB	

INDIANA
DEPARTMENT OF TRANSPORTATION

LAYOUT

HORIZONTAL SCALE	BRIDGE FILE
1" = 20'	64-00066 B
VERTICAL SCALE	DESIGNATION
1" = 10'	2003041
SURVEY BOOK	SHEETS
ELECTRONIC	8 of 24
CONTRACT	PROJECT
B-44107	2003041

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COMPOSITE PRESTRESSED CONCRETE
 BOX BEAM BRIDGE
 1 SPAN: 61'-0"
 30'-0" CLEAR ROADWAY; 25° SKEW LT.
 COUNTY ROAD 250 WEST OVER PHILLIPS DITCH
 PORTER COUNTY

Date: Sep 07, 2023, 1:05pm User Name: NGreer File: X:\Production\Files\2022\121-3027\PRJ-01\CAD\Plans\Layout\Porter No. 66.dwg

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CHECKED: EJJ	CHECKED: NB	

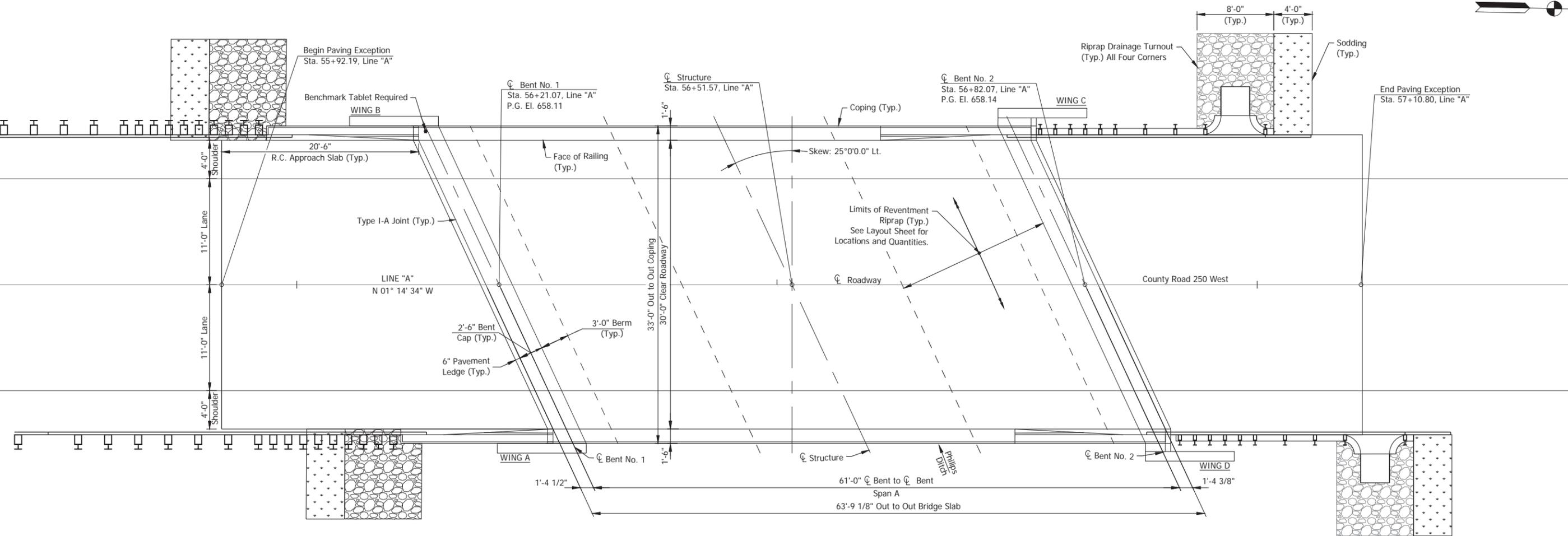
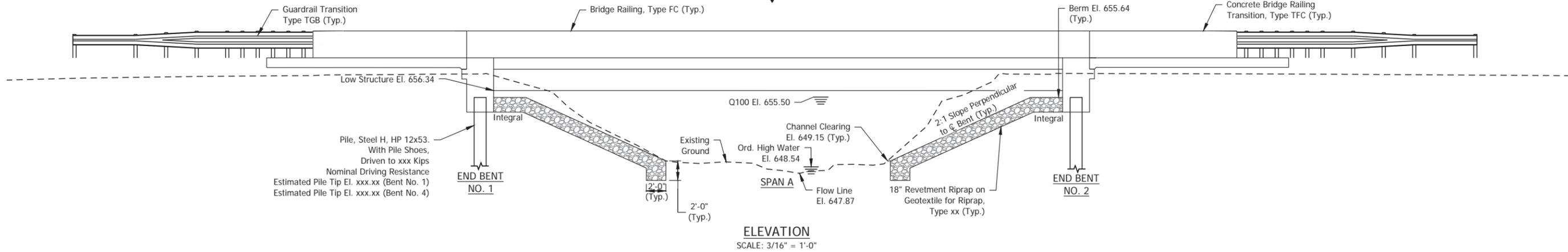
INDIANA DEPARTMENT OF TRANSPORTATION	
LAYOUT	

HORIZONTAL SCALE	BRIDGE FILE
1" = 20'	64-00066 B
VERTICAL SCALE	DESIGNATION
1" = 10'	2003041
SURVEY BOOK	SHEETS
ELECTRONIC	9 of 24
CONTRACT	PROJECT
B-44107	2003041

STRUCTURE TO BE BUILT
ON A 264' VERTICAL CURVE

PVI Sta. 56+46.00
PVI Elev. 661.18

+2.23% -2.00%



COMPOSITE PRESTRESSED CONCRETE
BOX BEAM BRIDGE
1 SPAN: 61'-0"
30'-0" CLEAR ROADWAY; 25° SKEW LT.
COUNTY ROAD 250 WEST OVER PHILLIPS DITCH
PORTER COUNTY

Date: Sep 07, 2023, 1:06pm User Name: NGreer File: X:\Production\Files\2023\121-3027\PRJ-01\CAD\Plans\General Sheet - 1 Porter No. 66.dwg

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: NB	DRAWN: NCG	
CHECKED: EJG	CHECKED: NB	

INDIANA
DEPARTMENT OF TRANSPORTATION

GENERAL PLAN

HORIZONTAL SCALE	BRIDGE FILE
AS NOTED	64-00066 B
VERTICAL SCALE	DESIGNATION
AS NOTED	2003041
SURVEY BOOK	SHEETS
ELECTRONIC	10 of 24
CONTRACT	PROJECT
B-44107	2003041

GENERAL NOTES

1. Reinforcing steel covering shall be 2 1/2" in top and 1" minimum in bottom of slab, 3" in footings, except bottom steel which shall be 4", and 2" in all other parts, unless noted.
2. See Layout Sheet for earthwork summary table.
3. Surface seal all exposed surfaces of new concrete barrier rail. Estimated quantity = xxxx sft.

DESIGN DATA

Designed for HL-93 loading, in accordance with AASHTO LRFD Bridge Design Specifications, Ninth Edition, 2020, and subsequent interims.

DEAD LOAD

Actual weight plus 35 lb/ft² for future wearing surface and 15 lb/ft² for permanent metal deck forms.

FLOOR SLAB

Designed with a 7 1/2" structural depth plus 1/2" sacrificial wearing surface.

DESIGN STRESSES

CONCRETE

Class C f_c = 4000 psi

REINFORCING STEEL

Grade 60 f_y = 60,000 psi

CONSTRUCTION LOADING

The exterior girder has been checked for strength, deflection, and overturning using the construction loads shown below. Cantilever overhang brackets were assumed for support of the deck overhang past the edge of the exterior girder. The finishing machine was assumed to be supported 6 in. outside the vertical coping form. The top overhang brackets were assumed to be located 6 in. past the edge of the vertical coping form. The bottom overhang brackets were assumed to be braced against the intersection of the girder bottom flange and web.

DECK FALSEWORK LOADS

Designed for 15 lb/ft² for permanent metal stay-in-place deck forms, removable deck forms, and 2-ft exterior walkway.

CONSTRUCTION LIVE LOAD

Designed for 20 lb/ft² extending 2-ft past the edge of coping and 75 lb/ft vertical force applied at a distance of 6 in. outside the face of coping over a 30-ft length of the deck centered with the finishing machine.

FINISHING-MACHINE LOAD

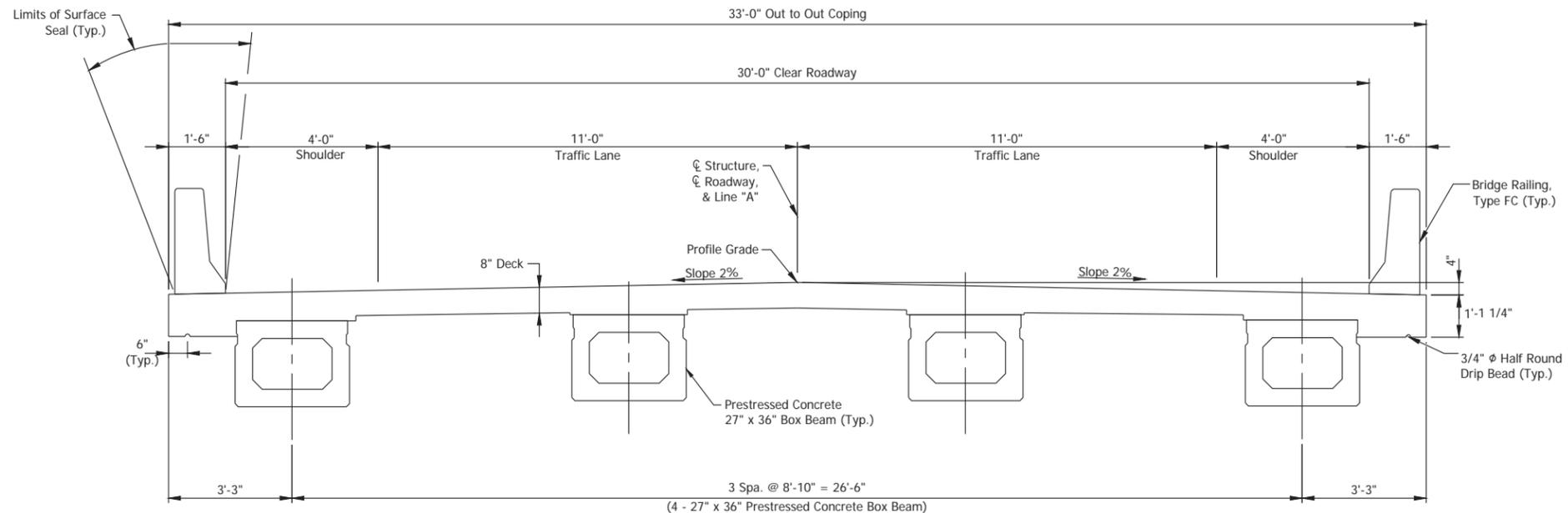
4500 lb distributed over 10 ft along the coping.

WIND LOAD

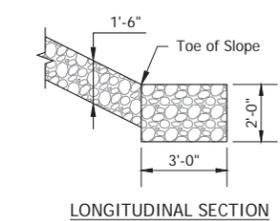
Designed for 70 mph horizontal wind loading in accordance with LRFD 3.8.1.

SEISMIC DESIGN LOAD

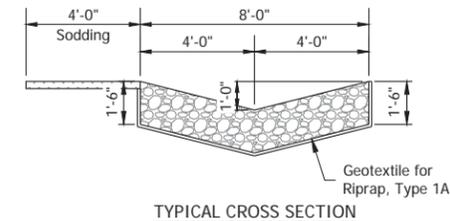
Seismic Design Category x
 Acceleration Coefficient xx
 Seismic Soil Profile Type Class x



TYPICAL SECTION
Scale 1/2" = 1'-0"



LONGITUDINAL SECTION



TYPICAL CROSS SECTION

RIPRAP TURNOUT DETAILS
Not to Scale

**COMPOSITE PRESTRESSED CONCRETE
 BOX BEAM BRIDGE
 1 SPAN: 61'-0"
 30'-0" CLEAR ROADWAY; 25° SKEW LT.
 COUNTY ROAD 250 WEST OVER PHILLIPS DITCH
 PORTER COUNTY**

Date: Sep 07, 2023, 1:06pm User Name: NGreer File: X:\Production\Files\2022\121-3027\PRJ-01\CAD\Plans\General Sheet 2 Porter No.66.dwg

RECOMMENDED FOR APPROVAL _____ DESIGN ENGINEER _____ DATE _____	INDIANA DEPARTMENT OF TRANSPORTATION	HORIZONTAL SCALE	BRIDGE FILE
		AS NOTED	64-00066 B
DESIGNED: _____ NB _____ DRAWN: _____ NCG _____	GENERAL PLAN	VERTICAL SCALE	DESIGNATION
		AS NOTED	2003041
CHECKED: _____ EJJ _____ CHECKED: _____ NB _____		SURVEY BOOK	SHEETS
		ELECTRONIC	11 of 24
		CONTRACT	PROJECT
		B-44107	2003041

ITEM	CONCRETE					RAILING, CONCRETE FC	REINF. BARS, EPOXY COATED	CONCRETE BRIDGE RAILING TRANSITION, TFC	BARRIER, DELINEATOR	REINF. CONC. BRIDGE APPROACH 12"	GEOTEXTILE FOR PAVEMENT, TYPE 2B	EXCAVATION, FOUNDATION, UNCLASSIFIED	AGGREGATE FOR END BENT BACKFILL	GEOTEXTILE FOR RIPRAP, TYPE 1A	PIPE, END BENT DRAIN, 6 IN	PIPE, ROADWAY DRAIN CASTING EXTENSION	CONC. STR. MEMBERS		THREADED TIE BAR ASSEMBLY, EPOXY COATED	TEST PILE, INDICATOR		PILE SHOE, HP 12 X 53	PILE, STEEL H HP 12 X 53	LONGITUDINAL GROOVING	SURFACE SEAL*	
	CLASS C	CLASS C	CLASS A	CLASS B	CLASS B												CONCRETE BOX BEAM, 27 IN. X 36 IN.	I-BEAM TYPE SIZE		PRODUCTION	RESTRIKE					
	SUPERSTR	SUBSTR	SUBSTR	ABOVE FTG.	IN FTG.												LFT	LFT		LFT	EACH					LFT
Superstructure		53.9					9,706											248		32					213.0	
End Bent No. 1		30.8					5,547			97	97	83	23	279.1	70						70	1	8	420		
End Bent No. 2		30.8					5,537			97	97	83	23	332	70						70	1	8	420		
R. C. Bridge Approach (12") at End Bent No. 1							5,820																		97.0	
R. C. Bridge Approach (12") at End Bent No. 2							5,820																		97.0	
Railing						12	5,500	-4	8																	1201
TOTALS		115.5				12	37,930	4	8	194	194	166	46	611.1	140			248		32	140	2	16	840	407	1201

* Estimated Quantity

Date: Sep 07, 2023, 1:06pm User Name: NGreer File: X:\Production\Files\2022\121-3027\PRJ-01\CAD\Plans\Bridges\Summary\Porter No. 66.dwg

RECOMMENDED FOR APPROVAL _____	DESIGN ENGINEER _____	DATE -- / -- / --
DESIGNED: _____ NB _____	DRAWN: _____ NCG _____	
CHECKED: _____ EJJ _____	CHECKED: _____ NB _____	

INDIANA
DEPARTMENT OF TRANSPORTATION

BRIDGE SUMMARY

HORIZONTAL SCALE	BRIDGE FILE
N/A	64-00066 B
VERTICAL SCALE	DESIGNATION
N/A	2003041
SURVEY BOOK	SHEETS
ELECTRONIC	12 of 24
CONTRACT	PROJECT
B-44107	2003041

Categorical Exclusion
Appendix C
Early Coordination



January 10, 2023

Sample Early
Coordination
Letter

Re: Early Coordination Letter
Des. No.: 2003041
County Bridge No. 66 - County Road (CR) 250 West over Phillips Ditch, Local Project
Porter County, Indiana

To whom it may concern:

Porter County and the Federal Highway Administration (FHWA), with oversight by the Indiana Department of Transportation (INDOT) LaPorte District, intend to proceed with the aforementioned bridge project along CR 250 W in Porter County (Des. No. 2003041). The proposed project involves Bridge No. 66, National Bridge Inventory (NBI) No. 6400044.

This letter is part of the early coordination phase of the environmental review. At this time, we are requesting comments from your area of expertise regarding any possible environmental effects (social and natural) associated with this project. **Please use the above Des. No. and project description in your reply.** Your comments will be incorporated into the formal environmental study. Your cooperation in this endeavor is appreciated.

Project Location and Existing Conditions

The proposed project is located in Porter County along CR 250 W approximately 1.5 miles south of SR 8. Specifically, the project is located in Section 21, Township 33 North, Range 6 West in Boone Township as depicted on the Kouts U.S. Geological Survey 1:24,000 scale quadrangle map. Adjacent land use is rural and consists of agriculture, streams, and residential areas.

CR 250 W is functionally classified as a rural major collector. The typical cross section of CR 250 W consists of two 10-foot wide travel lanes with no shoulder. The posted speed limit at the project location is 45 miles per hour. Bridge No. 66 was built in 1930, was reconstructed in 1996, and is a single-span prestressed concrete adjacent box beam bridge. The clear roadway width is 30.0 feet, out-to-out deck width is 30.6 feet, and the bridge length is 52.5 feet. Drainage is conveyed along the west side of CR 250 W via an unnamed tributary to Keller Ditch, which flows south into the project area and turns west. Phillips Ditch flows east to west underneath CR 250 W via Bridge No. 66.

The need for the project stems from the deteriorated condition of the existing bridge. According to the structure inspection report dated May 12, 2021, the condition ratings of the bridge deck, superstructure, and substructure were 6 ("satisfactory"), 6 ("satisfactory"), and 5 ("fair") out of 9, respectively. The bridge deck exhibited moderate seepage with efflorescence between beams. The superstructure exhibited efflorescence, beam deterioration, and widening. The substructure exhibited widening at the east abutment ends, deterioration at the north abutment including cracks and an offset footing, and spalling. The purpose of the project is to improve the overall condition of the structure and its components to a rating of at least 7 ("good") out of 9.

Proposed Project

The preferred alternative involves the removal of Bridge No. 66 and construction of a new replacement bridge over Phillips Ditch. The replacement structure is expected to be a single-span prestressed concrete

box beam bridge featuring two 11-foot wide travel lanes with 4-foot shoulders and standard concrete railing atop the bridge deck. The clear roadway width and out to out width will be 30 feet and 33 feet, respectively, and the bridge will be approximately 63.75 feet in length.

Roadway approach work is expected to include milling and hot mix asphalt (HMA) resurfacing that will extend approximately 277 feet south and 214 feet north of the bridge. The roadway section will include 11-foot lanes with 2- to 4-foot paved (3- to 5-foot usable) shoulders. Riprap drainage turnouts will be constructed in all four bridge quadrants. New guardrail will be constructed along both approaches to the bridge. Not including incidental construction, the total estimated length of the project is 610 feet (0.116 mile). An additional 50 feet of incidental construction is proposed at either end of the project for transitioning CR 250 West back into its existing profile.

The maintenance of traffic (MOT) plan will include a full closure with detour route. The preliminary detour route will utilize CR W 1700 N, CR 125 W (S Baums Bridge Road), and CR W 900 S. The MOT will be implemented per the *Indiana Design Manual* guidelines. Construction is anticipated to begin in summer of 2026.

Right-of-Way (ROW)

The existing ROW along CR 250 West extends 20 feet east and west of the road centerline (40 feet total width). This project is anticipated to require new permanent ROW from both sides of CR 250 W totaling greater than 0.5 acre. Tree clearing will likely be necessary.

Environmental Resources

A Red Flag Investigation (RFI) was performed within a 0.5-mile radius around the project area. Several “Red Flags” were identified, including various water resources; however, not all will impact the proposed project. These items will be monitored throughout project development.

Section 106 of the National Historic Preservation Act

Coordination will occur with INDOT Cultural Resources Office (CRO) to evaluate the project area for archaeological and historic resources and for Section 106 compliance. This project is expected to be eligible for evaluation under the Minor Projects Programmatic Agreement (MPPA) between INDOT, FHWA, State Historic Preservation Officer (SHPO), and the Advisory Council on Historic Preservation.

Range-wide Informal Programmatic Consultation

Porter County is within the range of the federally endangered Indiana bat (*Myotis sodalis*) and northern long-eared bat (NLEB) (*Myotis septentrionalis*). The project is anticipated to qualify for the U.S. Fish and Wildlife Service (USFWS) Range-wide Programmatic Informal Consultation for the Indiana bat and the NLEB.

Early Coordination

This letter is part of the early coordination review process. You are asked to review this information and provide any comments you may have relative to anticipated impacts of the project on areas in which you have jurisdiction or special expertise. We will incorporate your comments into a study of the project’s environmental impacts. To facilitate the development of this project, you are asked to reply within **30 calendar days** of receipt of this letter. However, should you find that an extension to the response time is necessary, a reasonable amount may be granted upon request.

If you have any questions regarding this project, please feel free to contact me at (574) 334-5491 or at CHoogewerf@lochgroup.com. Additionally, should you want to contact the sponsor of this project, Porter County, please contact Robert Thompson at 219-465-3540 or at rthompson@porterco.org.

Thank you in advance for your input.

Sincerely,



Carson J. Hoogewerf
Environmental Specialist
Lochmueller Group, Inc.

Attachments:

• General Location Map	
• USGS Quad Map	Removed to avoid duplication. See Appendix B.
• Aerial Map	
• Red Flag Investigation Maps	Removed to avoid duplication. See Appendix E.
• Photo Location Map and Project Photographs	Removed to avoid duplication. See Appendix B.

Distribution List:

- Federal Highway Administration - Indiana Division
- U.S. Department of Agriculture, Natural Resources Conservation Service
- Chicago Regional Office, U.S. Department of Housing and Urban Development
- National Park Service, Midwest Regional Office
- U.S. Army Corps of Engineers, Chicago District
- U.S. Coast Guard, Ninth District
- INDOT LaPorte District Office, Environmental Section Manager
- INDOT LaPorte District Office, Project Manager
- INDOT Environmental Services Division
- IDNR, Division of Fish and Wildlife
- IDEM, Wetlands and Stormwater Programs
- Northwestern Indiana Regional Planning Commission
- Indiana Geological and Water Survey
- Porter County Highway Department
- Porter County Stormwater Advisory Board (formerly Drainage Board)
- Porter County Board of Commissioners
- Porter County Council
- Porter County Township Trustees, Boone Township
- Porter County Surveyor's Office
- Porter County Emergency Management Agency
- Porter County E911
- Porter County Sheriff Department

- Porter County MS4 Coordinator
- Porter County Floodplain Administrator
- Northwest Health - Porter Ambulance Service
- Kouts Police Department
- Kouts Fire Department
- Metropolitan School District of Boone Township



Organization and Project Information

Project ID:
Des. ID: 2003041
Project Title: CR 250 W over Phillips Ditch Bridge Project
Name of Organization: Lochmueller Group, Inc.
Requested by: Carson Hoogewerf

Environmental Assessment Report

1. Geological Hazards:

- High liquefaction potential
- 1% Annual Chance Flood Hazard

2. Mineral Resources:

- Bedrock Resource: Low Potential
- Sand and Gravel Resource: High Potential

3. Active or abandoned mineral resources extraction sites:

- None documented in the area

*All map layers from Indiana Map (maps.indiana.edu)

DISCLAIMER:

This document was compiled by Indiana University, Indiana Geological Survey, using data believed to be accurate; however, a degree of error is inherent in all data. This product is distributed "AS-IS" without warranties of any kind, either expressed or implied, including but not limited to warranties of suitability to a particular purpose or use. No attempt has been made in either the design or production of these data and document to define the limits or jurisdiction of any federal, state, or local government. The data used to assemble this document are intended for use only at the published scale of the source data or smaller (see the metadata links below) and are for reference purposes only. They are not to be construed as a legal document or survey instrument. A detailed on-the-ground survey and historical analysis of a single site may differ from these data and this document.

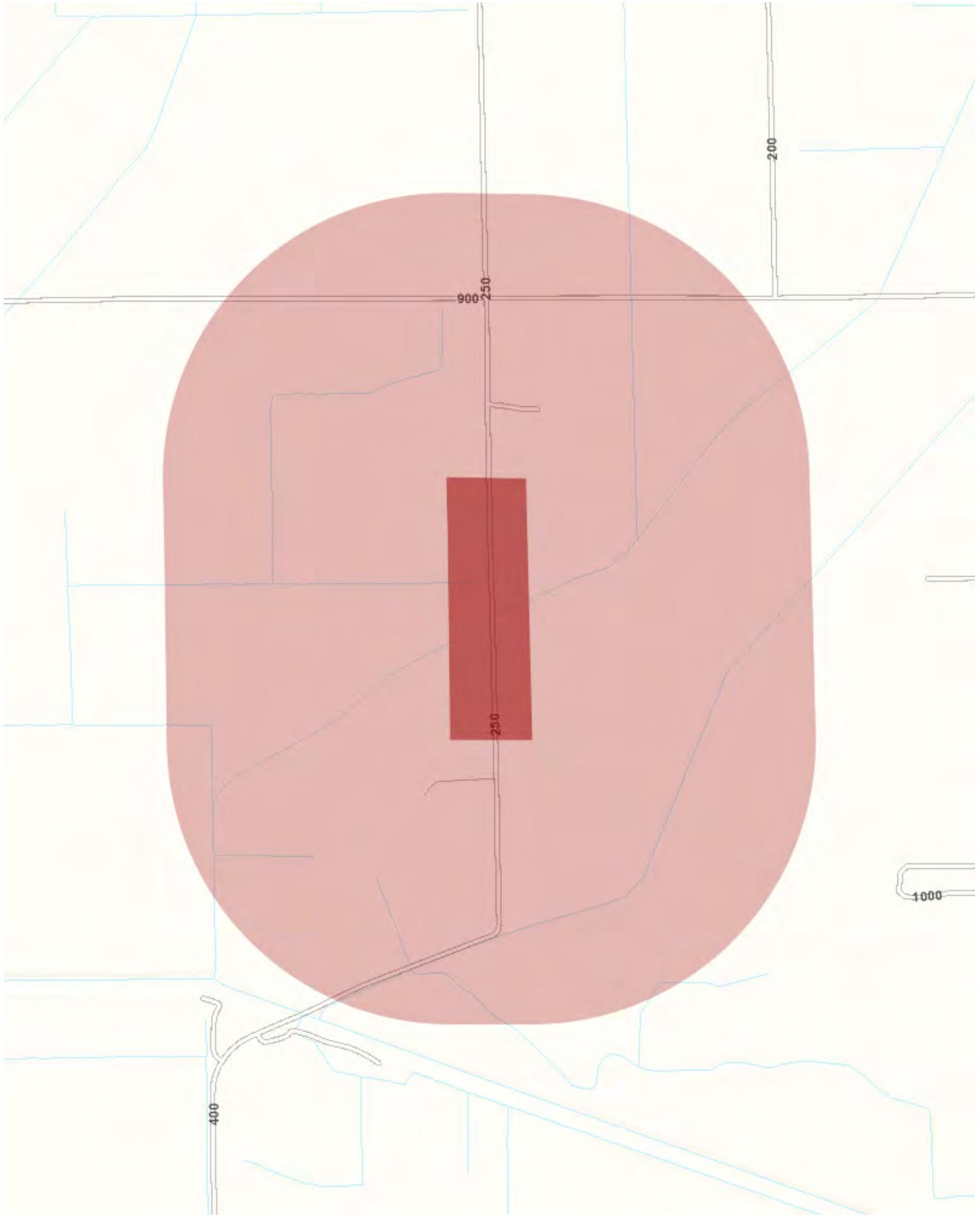
This information was furnished by Indiana Geological Survey

Address: 420 N. Walnut St., Bloomington, IN 47404

Email: IGSEnvir@indiana.edu

Phone: 812 855-7428

Date: January 10, 2023





TELEPHONE RECORD

Date of Call:	January 10, 2023	Phone Number:	219-996-4771
		Conversation With:	Linda Harbit, Transportation Director
Submitted By:	Carson Hoogewerf	Company Name:	Metropolitan School District of Boone Township
Copies To:	N/A	Project:	CR 250 W over Phillips Ditch Bridge Project (Des. No. 2003041)
Subject:	MOT Concerns for Bus Route		

Remarks:

Linda Harbit, Transportation Director with Metropolitan School District of Boone Township, contacted Carson Hoogewerf with Lochmueller Group via phone at approximately 3:45 PM ET on January 10, 2023 in reference to the Porter County Bridge No. 66 (CR 250 W over Phillips Ditch) Bridge Project (Des. No. 2003041).

Linda indicated that she has a bus stop located approximately 0.1 mile south of the subject bridge, and that closure of the bridge and associated detour would likely interfere with the existing bus route. Linda expressed concern that the closure would necessitate the bus approaching the stop from the south, adding an additional 10 to 15 minutes of travel time, and would require the bus to turn around after reaching the stop.

Linda inquired about the timing of the proposed construction. Carson responded that construction could occur in late 2024 or early (spring) 2025. Linda requested that, if possible, the detour be instated in the summertime when bus routes discontinue, or that as much of the detour duration as possible occur in the summer time.

Carson indicated that he would record Linda's concerns and pass them onto the design team for consideration into the Maintenance of Traffic plan, and that Lochmueller Group would follow up with her with any updates to the project resulting from her concerns.

112 West Jefferson Blvd, Suite 500
South Bend, Indiana 46601
PHONE: 574.334.5460

Carson Hoogewerf

From: Linda Harbit [REDACTED]
Sent: Friday, January 13, 2023 9:22 AM
To: Carson Hoogewerf
Subject: Re: Early Coordination - CR 250 W over Phillips Ditch - Des. No. 2003041
Attachments: 20230110_Transportation Director_Telephone Record.docx

EXTERNAL

I am good with this.
Last student day of 2024 last day of school would be May 30th
We should start school in the fall approx august 15?? of course too soon for that calendar to come out
Thank you so much for this consideration.

On Wednesday, January 11, 2023 at 07:54:15 AM CST, Carson Hoogewerf <choogewerf@lochgroup.com> wrote:

Good morning Linda,

Thank you for reaching out yesterday in regard to this future bridge project in Porter County (Des. No. 2003041). Attached to this email is a record of our phone conversation – please review that document and verify that that your concerns are present and accurately represented. If any changes are needed, let me know and I can adjust. Once you've verified the information, I will hand it over to our design team for consideration, and your comments will be incorporated into our environmental review documentation.

I have one additional question that will help our design team with your concerns: What is the approximate school schedule for the route in question? – i.e. what months does school get out and return?

Thank you for your engagement with this project, and of course, if you have any additional concerns please do not hesitate to reach out.

Sincerely,

Carson Hoogewerf



Carson Hoogewerf
Environmental Specialist I

Carson Hoogewerf

From: Michels, Stewart <SMichels@indot.IN.gov>
Sent: Thursday, January 12, 2023 11:32 AM
To: Carson Hoogewerf
Subject: RE: Des. No. 2003041 - CR 250 W over Phillips Ditch Bridge Project - Early Coordination Letter

EXTERNAL

Carson,

Thank you for providing the early coordination letter for Des2003041 in Porter County to the LaPorte District – Environmental Services. We do not have any comments at this time.

Best,
Stew

Stewart Michels

Environmental Manager Supervisor

315 East Boyd Boulevard
LaPorte, IN 46350

Office: (219) 325-7560

Cell: (219) 402-7315

Email: SMichels@indot.in.gov



From: Carson Hoogewerf <CHoogewerf@lochgroup.com>

Sent: Tuesday, January 10, 2023 9:45 AM

Cc: Krueckeberg, John <JKrueckeberg@indot.IN.gov>; rthompson@porterco.org; Chad Costa <ccosta@lochgroup.com>; Jennifer Pittman <JPittman@lochgroup.com>

Subject: Des. No. 2003041 - CR 250 W over Phillips Ditch Bridge Project - Early Coordination Letter

**** This is an EXTERNAL email. Exercise caution. DO NOT open attachments or click links from unknown senders or unexpected email. ****

Good morning,

Please find attached the early coordination letter for this Local Public Agency Bridge Project located in Porter County, Indiana (Des. No. 2003041). Please contact me at your earliest convenience should you have any questions or comments regarding this project.

Thank you,



Carson Hoogewerf
Environmental Specialist I

Carson Hoogewerf

From: Hall, Soren G CIV USARMY CELRC (USA) <Soren.G.Hall@usace.army.mil>
Sent: Thursday, January 19, 2023 11:17 AM
To: rthompson@porterco.org
Cc: jkrueckeberg@indot.in.gov; Carson Hoogewerf; Jennifer Pittman
Subject: USACE Comments - LRC-2023-036 - Pre-App - Porter - County Road 250 Bridge No. 66 Replacement over Phillips Ditch (2003041)

EXTERNAL

INDOT Project:
Des. No.: 2003041
County Bridge No. 66 - County Road (CR) 250 West over Phillips Ditch, Local Project
Porter County, Indiana

Good Morning,

Thank you for including our agency on the early coordination letter received January 10, 2023. It appears that aquatic resources may exist on the proposed project site. A permit authorizing impacts to waters of the U.S. may be needed before proceeding with the project. If you have further questions, please contact our office for a pre-application meeting and reference project number LRC-2023-36.

Sincerely,

Soren G. Hall
Workflow Team Leader / Senior Project Manager
U.S. Army Corps of Engineers
Chicago District – Regulatory Branch
231 South LaSalle Street, 15th Floor
Chicago, Illinois 60604
Mobile: (312) 730-8839
<https://www.lrc.usace.army.mil/Missions/Regulatory/>

From: Hall, Soren G CIV USARMY CELRC (USA)
Sent: Thursday, January 12, 2023 11:19 AM
To: rthompson@porterco.org
Cc: jkrueckeberg@indot.in.gov; choogewerf@lochgroup.com; jpittman@lochgroup.com
Subject: USACE Request Assigned - LRC-2023-036 - Pre-App - Porter - County Road 250 Bridge No. 66 Replacement over Phillips Ditch (2003041)

Good Morning,

Your request, file number LRC-2022-36 has been assigned. I will be your Project Manger. If additional information is requested during the review of your submittal, please email all documents directly to your Project Manager.

The Regulatory Branch is committed to providing the highest level of Customer Service. We value your comments and would appreciate you contacting our management team if you have any comments/concerns regarding our customer

service. You can reach the Branch Chief at Teralyn.R.Murray@usace.army.mil or the Assistant Branch Chief at Kathy.G.Chernich@usace.army.mil.

Sincerely,

Soren G. Hall
Workflow Team Leader
U.S. Army Corps of Engineers
Chicago District – Regulatory Branch
231 South LaSalle Street, 15th Floor
Chicago, Illinois 60604
Mobile: (312) 730-8839
<https://www.lrc.usace.army.mil/Missions/Regulatory/>

From: Carson Hoogewerf <CHoogewerf@lochgroup.com>
Sent: Tuesday, January 10, 2023 9:45 AM
Cc: Krueckeberg, John <jkrueckeberg@indot.in.gov>; rthompson@porterco.org; Chad Costa <CCosta@lochgroup.com>; Jennifer Pittman <JPittman@lochgroup.com>
Subject: [URL Verdict: Neutral][Non-DoD Source] Des. No. 2003041 - CR 250 W over Phillips Ditch Bridge Project - Early Coordination Letter

Good morning,

Please find attached the early coordination letter for this Local Public Agency Bridge Project located in Porter County, Indiana (Des. No. 2003041). Please contact me at your earliest convenience should you have any questions or comments regarding this project.

Thank you,



 **Web:** <http://lochgroup.com>
   

Carson Hoogewerf

Environmental Specialist I

 **Lochmueller Group**
112 W Jefferson Blvd, Suite 500, South Bend, IN 46601

 **Email:** CHoogewerf@lochgroup.com

Direct: 574.334.5491
 **Mobile:** 219.789.4661

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U.S. Department of
Homeland Security

**United States
Coast Guard**



Commander
Ninth Coast Guard District

1240 E 9th St
Cleveland, OH 44199
Staff Symbol: (dpb)
Phone: (216) 902-6086
Fax: (216) 902-6088
Email: william.b.stanifer@uscg.mil

16590
January 17, 2023
B-016/mow

Carson Hoogewerf
Lochmueller Group, Inc.
112 W. Jefferson Blvd., Suite 500
South Bend, Indiana 46601

Dear Carson:

This letter responds to your January 10, 2023 letter sent on behalf of the Indiana Department of Transportation requesting a jurisdictional determination under 33 C.F.R. § 2.36(a) and inquiry regarding the need for a Coast Guard bridge permit for proposed replacement of County Road 250 West bridge over Phillips Ditch in Porter County, Indiana (Des. No. 2003041).

Upon review of our records, we confirmed that the Coast Guard has not previously determined the navigability of this location. The location of the current and proposed structure is not located over any waterway over which the Coast Guard would exercise its jurisdiction as it pertains to its bridge permitting authorities. Due to these factors, the Coast Guard does not intend to make a navigability determination in accordance with 14 U.S.C. § 563.

Absent such a determination, the Coast Guard hereby declines to assert jurisdiction under our bridge authorities on the above-described waterway or reach thereof. As such, a Coast Guard bridge permit is not required for the subject bridge project and other associated permit conditions related to lighting, navigational clearances, etc. will not be prescribed by our office.

This decision relates only to Coast Guard bridge permitting and does not relieve the applicant of the responsibility to comply with any other applicable federal, state or local laws and regulations that may apply to this project. I recommend you contact the U.S. Army Corps of Engineers Detroit District to determine whether they have any permitting requirements on the waterway.

Please contact this office prior to construction of any other bridges on this waterway as this determination may change based upon future waterway usage.

If you have any questions about this matter, please contact Mr. Michael Walker at (216) 902-6087 or michael.o.walker2@uscg.mil.

Sincerely,

STANIFER.WILLIAM.B.
M.B.1053987350

Digitally signed by
STANIFER.WILLIAM.B.10539873
50
Date: 2023.01.17 12:16:46 -05'00'

W. B. STANIFER
Chief, Bridge Branch
U. S. Coast Guard
By direction

January 19, 2023

Carson Hoogewerf
Lochmueller Group
112 West Jefferson Street, Suite 500
South Bend, Indiana 46601

Dear Mr. Hoogewerf:

The proposed Local Public Agency bridge project in Porter County, Indiana, (Des. No. 2003041) as referred to in your letter received January 10, 2023, will cause a conversion of prime farmland.

The attached packet of information is for your use competing Parts VI and VII of the AD-1006. After completion, the federal funding agency needs to forward one copy to NRCS for our records.

If you need additional information, please contact John Allen at 317-295-5859 or john.allen@usda.gov.

Sincerely,

JOHN ALLEN Digitally signed by JOHN ALLEN
Date: 2023.01.19 11:25:31 -05'00'

JOHN ALLEN
State Soil Scientist

Enclosers

FARMLAND CONVERSION IMPACT RATING

PART I (To be completed by Federal Agency)		Date Of Land Evaluation Request 1/10/2023			
Name of Project CR 250 W over Phillips Ditch, Des. No. 2003041		Federal Agency Involved FHWA			
Proposed Land Use Transportation		County and State Porter County, IN			
PART II (To be completed by NRCS)		Date Request Received By NRCS 1/10/23		Person Completing Form: JRA	
Does the site contain Prime, Unique, Statewide or Local Important Farmland? <i>(If no, the FPPA does not apply - do not complete additional parts of this form)</i>		YES <input checked="" type="checkbox"/>	NO <input type="checkbox"/>	Acres Irrigated	Average Farm Size
					275
Major Crop(s) Corn	Farmable Land In Govt. Jurisdiction Acres: 234883% 88	Amount of Farmland As Defined in FPPA Acres: 20715% 77			
Name of Land Evaluation System Used LESA	Name of State or Local Site Assessment System	Date Land Evaluation Returned by NRCS 1/19/23			
PART III (To be completed by Federal Agency)		Alternative Site Rating			
		Site A	Site B	Site C	Site D
A. Total Acres To Be Converted Directly		0.66			
B. Total Acres To Be Converted Indirectly		0.0			
C. Total Acres In Site		12.1			
PART IV (To be completed by NRCS) Land Evaluation Information					
A. Total Acres Prime And Unique Farmland		0.50			
B. Total Acres Statewide Important or Local Important Farmland		0.00			
C. Percentage Of Farmland in County Or Local Govt. Unit To Be Converted		<0.001			
D. Percentage Of Farmland in Govt. Jurisdiction With Same Or Higher Relative Value		63			
PART V (To be completed by NRCS) Land Evaluation Criterion Relative Value of Farmland To Be Converted (Scale of 0 to 100 Points)		79			
PART VI (To be completed by Federal Agency) Site Assessment Criteria <i>(Criteria are explained in 7 CFR 658.5 b. For Corridor project use form NRCS-CPA-106)</i>		Maximum Points	Site A	Site B	Site C
1. Area In Non-urban Use		(15)	15		
2. Perimeter In Non-urban Use		(10)	10		
3. Percent Of Site Being Farmed		(20)	0		
4. Protection Provided By State and Local Government		(20)	0		
5. Distance From Urban Built-up Area		(15)	15		
6. Distance To Urban Support Services		(15)	10		
7. Size Of Present Farm Unit Compared To Average		(10)	5		
8. Creation Of Non-farmable Farmland		(10)	0		
9. Availability Of Farm Support Services		(5)	5		
10. On-Farm Investments		(20)	10		
11. Effects Of Conversion On Farm Support Services		(10)	0		
12. Compatibility With Existing Agricultural Use		(10)	0		
TOTAL SITE ASSESSMENT POINTS		160	70	0	0
PART VII (To be completed by Federal Agency)					
Relative Value Of Farmland (From Part V)		100	79	0	0
Total Site Assessment (From Part VI above or local site assessment)		160	70	0	0
TOTAL POINTS (Total of above 2 lines)		260	149	0	0
Site Selected: Site A		Date Of Selection January 24, 2023		Was A Local Site Assessment Used?	
				YES <input type="checkbox"/>	NO <input checked="" type="checkbox"/>
Reason For Selection: Site A presented the least amount of impacts while accomplishing the purpose and need of the project					
Name of Federal agency representative completing this form: Carson Hoogewerf on behalf of FHWA					Date: 1/24/23

(See Instructions on reverse side)

Form AD-1006 (03-02)

State of Indiana
DEPARTMENT OF NATURAL RESOURCES
Division of Fish and Wildlife
Early Coordination/Environmental Assessment

DNR #: ER-25252

Request Received: January 10, 2023

Requestor: Lochmueller Group
Carson Hoogewerf
112 West Jefferson Boulevard, Suite 500
South Bend, IN 46601

Project: CR 250 West bridge (NBI #6400044) replacement over Phillips Ditch, 1.5 miles south of SR 8; Des #2003041

County/Site info: Porter

The Indiana Department of Natural Resources has reviewed the above referenced project per your request. Our agency offers the following comments for your information and in accordance with the National Environmental Policy Act of 1969.

If our agency has regulatory jurisdiction over the project, the recommendations contained in this letter may become requirements of any permit issued. If we do not have permitting authority, all recommendations are voluntary.

Regulatory Assessment: This proposal will require the formal approval of our agency for construction in a floodway pursuant to the Flood Control Act (IC 14-28-1), unless it qualifies for a bridge exemption (see enclosure). Please include a copy of this letter with the permit application if the project does not meet the bridge exemption criteria.

Natural Heritage Database: The Natural Heritage Program's data have been checked. To date, no plant or animal species listed as state or federally threatened, endangered, or rare have been reported to occur in the project vicinity.

Fish & Wildlife Comments: Avoid and minimize impacts to fish, wildlife, and botanical resources to the greatest extent possible, and compensate for impacts. The following are recommendations that address potential impacts identified in the proposed project area:

1) Crossing Structure & Wildlife Passage:
Maintaining or improving fish and wildlife passage at existing or proposed crossing locations is a priority for the Division of Fish & Wildlife (DFW) to reduce wildlife mortality along roadways. The DFW has outlined different requirements for different types of crossing structure impacts. For crossing replacements, the new structure must include wildlife passage appropriate for the type of replacement structure being proposed. If the replacement structure is sized to accommodate white-tailed deer passage then it should be included in the design of the new structure. If white-tailed deer passage is not possible with the existing structure, deer passage still needs to be considered in the design and at minimum the bank lines must be restored within structures to allow for smaller wildlife passage above the ordinary high water mark. All wildlife passage designs must include a smooth level pathway a minimum of 1-2 feet in width composed of natural substrate (soil, sand, gravel, etc.) or compacted aggregate fill over riprap (#2, #53, #73, etc.) tied into existing elevations both upstream and downstream. The stream crossing repairs or modifications, and any bank stabilization under or around the structure, must not create conditions that are less favorable for wildlife passage when compared to existing conditions. Upgrading wildlife passage for rehabilitated/modified structures is encouraged whenever possible to improve wildlife/vehicle safety.

There are a number of techniques and materials for incorporating wildlife passage into the design of a crossing structure. Coordination with a Regional Environmental Biologist to address wildlife passage issues before submitting a permit application (if required) is

Attachments: A - Bridge Exemption Criteria

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DEPARTMENT OF NATURAL RESOURCES
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Early Coordination/Environmental Assessment

encouraged to avoid delays in the permitting process. The following links are good resources to consider in the design of stream crossing structures to maintain fish and wildlife passage:

<https://www.fs.usda.gov/ccrc/tool/fishxing-fish-passage-learning-systems>,
<https://www.fs.usda.gov/wildlifecrossings/library/index.php>,
https://www.fhwa.dot.gov/clas/ctip/wildlife_crossing_structures/,
<https://www.fhwa.dot.gov/engineering/hydraulics/pubs/11008/hif11008.pdf>.

For purposes of maintaining fish and wildlife passage through a crossing structure, the Environmental Unit recommends bridges rather than culverts and bottomless culverts rather than box or pipe culverts. Wide culverts are better than narrow culverts, and culverts with shorter through lengths are better than culverts with longer through lengths. If box or pipe culverts are used, the bottoms should be buried a minimum of 6" (or 20% of the culvert height/pipe diameter, whichever is greater up to a maximum of 2') below the stream bed elevation to allow a natural streambed to form within or under the crossing structure. Crossings should: span the entire channel width (a minimum of 1.2 times the OHWM width); maintain the natural stream substrate within the structure; and have stream depth, channel width, and water velocities during low-flow conditions that are approximate to those in the natural stream channel.

2) Bank Stabilization:

Establishing vegetation along the banks is critical for stabilization and erosion control. In addition to vegetation, some other form of bank stabilization may be needed. While hard armoring alone (e.g. riprap or glacial stone) may be needed in certain instances, soft armoring and bioengineering techniques should be considered first. In many instances, one or more methods are necessary to increase the likelihood of vegetation establishment. Combining vegetation with most bank stabilization methods can provide additional bank protection and help reduce impacts upon fish and wildlife. Information about bioengineering techniques can be found at <http://www.in.gov/legislative/iac/20120404-IR-312120154NRA.xml.pdf>. Also, the following is a USDA/NRCS document that outlines many different bioengineering techniques for streambank stabilization: https://efotg.sc.egov.usda.gov/references/public/IA/Chapter-16_Streambank_and_Shoreline_Protection.pdf.

Riprap must not be placed in the active thalweg channel or placed in the streambed in a manner that precludes fish or aquatic organism passage (riprap must not be placed above the existing streambed elevation). Riprap may be used only at the toe of the sideslopes up to the ordinary high water mark (OHWM). The banks above the OHWM must be restored, stabilized, and revegetated using geotextiles and a mixture of grasses, sedges, wildflowers, shrubs, and trees native to Northern Indiana and specifically for stream bank/floodway stabilization purposes as soon as possible upon completion.

3) Riparian Habitat:

We recommend a mitigation plan be developed (and submitted with the permit application, if required) for any unavoidable habitat impacts that will occur. The DNR's Habitat Mitigation Guidelines (and plant lists) can be found online at: <https://www.in.gov/nrc/files/IB-17.pdf>.

Impacts to non-wetland forest of one (1) acre or more in a rural or urban area should be mitigated at a minimum 2:1 ratio based on area of impact. Impacts to non-wetland forest under one (1) acre but at least 0.10 acre in a rural or urban area should be mitigated at a minimum 1:1 ratio based on area of impact. Impacts under 0.10 acre in a rural area typically do not require mitigation or additional plantings beyond seeding and stabilizing disturbed areas, though there are exceptions for high quality habitat sites. Impacts

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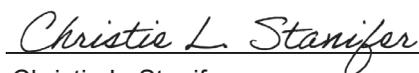
under 0.10 acre in an urban area should be mitigated by replacing trees that are 10" diameter-at-breast height (dbh) or greater by planting five trees, 1" to 2" in dbh, for each tree which is removed that is 10" dbh or greater. Seeding and stabilizing disturbed areas is required regardless of the impact amount and location. If floodway impacts to forested wetland and non-wetland habitat areas combine to be 0.10 acres or more, mitigation should be done and coordinated with the biologist, as needed.

The additional measures listed below should be implemented to avoid, minimize, or compensate for impacts to fish, wildlife, and botanical resources:

1. Revegetate all bare and disturbed areas within the project area using a mixture of grasses (excluding all varieties of tall fescue), sedges and wildflowers native to Northern Indiana and specifically for stream bank/floodway stabilization purposes as soon as possible upon completion. A native herbaceous seed mixture must include at least 5 species of grasses and sedges and 5 species of wildflowers.
2. Do not work in the waterway from April 1 through June 30 without the prior written approval of the Division of Fish and Wildlife.
3. Do not cut any trees suitable for Indiana bat or Northern Long-eared bat roosting (greater than 5 inches dbh, living or dead, with loose hanging bark, or with cracks, crevices, or cavities) from April 1 through September 30.
4. Operate equipment used to replace the bridge from the existing roadway.
5. Use minimum average 6 inch graded riprap stone extended below the normal water level to provide habitat for aquatic organisms in the voids.
6. Do not use broken concrete as riprap.
7. Underlay the riprap with a bedding layer of well graded aggregate or a geotextile to prevent piping of soil underneath the riprap.
8. Minimize the movement of resuspended bottom sediment from the immediate project area.
9. Do not deposit or allow construction/demolition materials or debris to fall or otherwise enter the waterway. Any incidental fallen material or debris in the waterway must be removed within 24 hours using best management practices, particularly lifting material out of the waterway and not dragging it across the streambed whenever possible.
10. Appropriately designed measures for controlling erosion and sediment must be implemented to prevent sediment from entering the waterbody or leaving the construction site; maintain these measures until construction is complete and all disturbed areas are stabilized.
11. Seed and protect all disturbed streambanks and slopes not protected by other methods that are 3:1 or steeper with erosion control blankets that are heavy-duty, biodegradable, and net free or that use loose-woven / Leno-woven netting to minimize the entrapment and snaring of small-bodied wildlife such as snakes and turtles (follow manufacturer's recommendations for selection and installation); seed and apply mulch on all other disturbed areas.
12. Do not excavate or place fill in any riparian wetland.

Contact Staff:

Christie L. Stanifer, Environ. Coordinator, Fish & Wildlife
Our agency appreciates this opportunity to be of service. Please contact the above staff member at (317) 232-4080 if we can be of further assistance.



Christie L. Stanifer
Environ. Coordinator
Division of Fish and Wildlife

Date: February 8, 2023

Attachments: A - Bridge Exemption Criteria



United States Department of the Interior



FISH AND WILDLIFE SERVICE
Indiana Ecological Services Field Office
620 South Walker Street
Bloomington, IN 47403-2121
Phone: (812) 334-4261 Fax: (812) 334-4273

In Reply Refer To:

August 01, 2023

Project Code: 2023-0026602

Project Name: Des. No. 2003041, CR 250 W over Phillips Ditch, Bridge Project, Porter County, Indiana

Subject: List of threatened and endangered species that may occur in your proposed project location or may be affected by your proposed project

To Whom It May Concern:

The enclosed species list identifies threatened, endangered, proposed and candidate species, as well as proposed and final designated critical habitat, that may occur within the boundary of your proposed project and/or may be affected by your proposed project. The species list fulfills the requirements of the U.S. Fish and Wildlife Service (Service) under section 7(c) of the Endangered Species Act (Act) of 1973, as amended (16 U.S.C. 1531 *et seq.*).

New information based on updated surveys, changes in the abundance and distribution of species, changed habitat conditions, or other factors could change this list. Please feel free to contact us if you need more current information or assistance regarding the potential impacts to federally proposed, listed, and candidate species and federally designated and proposed critical habitat. Please note that under 50 CFR 402.12(e) of the regulations implementing section 7 of the Act, the accuracy of this species list should be verified after 90 days. This verification can be completed formally or informally as desired. The Service recommends that verification be completed by visiting the ECOS-IPaC website at regular intervals during project planning and implementation for updates to species lists and information. An updated list may be requested through the ECOS-IPaC system by completing the same process used to receive the enclosed list.

The purpose of the Act is to provide a means whereby threatened and endangered species and the ecosystems upon which they depend may be conserved. Under sections 7(a)(1) and 7(a)(2) of the Act and its implementing regulations (50 CFR 402 *et seq.*), Federal agencies are required to utilize their authorities to carry out programs for the conservation of threatened and endangered species and to determine whether projects may affect threatened and endangered species and/or designated critical habitat.

Please use the species list provided and visit the U.S. Fish and Wildlife Service's Region 3 Section 7 Technical Assistance website at - <http://www.fws.gov/midwest/endangered/section7/>

[s7process/index.html](#). This website contains step-by-step instructions which will help you determine if your project will have an adverse effect on listed species and will help lead you through the Section 7 process. For all **wind energy projects and projects that include installing towers that use guy wires or are over 200 feet in height**, please contact this field office directly for assistance, even if no federally listed plants, animals or critical habitat are present within your proposed project or may be affected by your proposed project.

A Biological Assessment is required for construction projects (or other undertakings having similar physical impacts) that are major Federal actions significantly affecting the quality of the human environment as defined in the National Environmental Policy Act (42 U.S.C. 4332(2)(c)). For projects other than major construction activities, the Service suggests that a biological evaluation similar to a Biological Assessment be prepared to determine whether the project may affect listed or proposed species and/or designated or proposed critical habitat. Recommended contents of a Biological Assessment are described at 50 CFR 402.12.

If a Federal agency determines, based on the Biological Assessment or biological evaluation, that listed species and/or designated critical habitat may be affected by the proposed project, the agency is required to consult with the Service pursuant to 50 CFR 402. In addition, the Service recommends that candidate species, proposed species and proposed critical habitat be addressed within the consultation. More information on the regulations and procedures for section 7 consultation, including the role of permit or license applicants, can be found in the "Endangered Species Consultation Handbook" at:

<http://www.fws.gov/endangered/esa-library/pdf/TOC-GLOS.PDF>

Migratory Birds: In addition to responsibilities to protect threatened and endangered species under the Endangered Species Act (ESA), there are additional responsibilities under the Migratory Bird Treaty Act (MBTA) and the Bald and Golden Eagle Protection Act (BGEPA) to protect native birds from project-related impacts. Any activity, intentional or unintentional, resulting in take of migratory birds, including eagles, is prohibited unless otherwise permitted by the U.S. Fish and Wildlife Service (50 C.F.R. Sec. 10.12 and 16 U.S.C. Sec. 668(a)). For more information regarding these Acts see <https://www.fws.gov/birds/policies-and-regulations.php>.

The MBTA has no provision for allowing take of migratory birds that may be unintentionally killed or injured by otherwise lawful activities. It is the responsibility of the project proponent to comply with these Acts by identifying potential impacts to migratory birds and eagles within applicable NEPA documents (when there is a federal nexus) or a Bird/Eagle Conservation Plan (when there is no federal nexus). Proponents should implement conservation measures to avoid or minimize the production of project-related stressors or minimize the exposure of birds and their resources to the project-related stressors. For more information on avian stressors and recommended conservation measures see <https://www.fws.gov/birds/bird-enthusiasts/threats-to-birds.php>.

In addition to MBTA and BGEPA, Executive Order 13186: *Responsibilities of Federal Agencies to Protect Migratory Birds*, obligates all Federal agencies that engage in or authorize activities that might affect migratory birds, to minimize those effects and encourage conservation measures that will improve bird populations. Executive Order 13186 provides for the protection of both migratory birds and migratory bird habitat. For information regarding the implementation of

Executive Order 13186, please visit <https://www.fws.gov/birds/policies-and-regulations/executive-orders/e0-13186.php>.

We appreciate your concern for threatened and endangered species. The Service encourages Federal agencies to include conservation of threatened and endangered species into their project planning to further the purposes of the Act. **Please include the Consultation Code in the header of this letter with any request for consultation or correspondence about your project that you submit to our office.**

Attachment(s):

- Official Species List
- Migratory Birds
- Wetlands

OFFICIAL SPECIES LIST

This list is provided pursuant to Section 7 of the Endangered Species Act, and fulfills the requirement for Federal agencies to "request of the Secretary of the Interior information whether any species which is listed or proposed to be listed may be present in the area of a proposed action".

This species list is provided by:

Indiana Ecological Services Field Office

620 South Walker Street

Bloomington, IN 47403-2121

(812) 334-4261

PROJECT SUMMARY

Project Code: 2023-0026602
 Project Name: Des. No. 2003041, CR 250 W over Phillips Ditch, Bridge Project, Porter County, Indiana
 Project Type: Bridge - Replacement
 Project Description: Porter County, with funding from the Federal Highway Administration (FHWA) and oversight from the Indiana Department of Transportation (INDOT), intends to proceed with the following bridge project along County Road (CR) 250 W approximately 1.5 miles south of State Road 8 in Porter County, Indiana.

The proposed project involves the replacement of the existing bridge, Bridge No. 64-00066. The replacement structure is expected to be a single-span prestressed concrete box beam bridge featuring two 11-foot wide travel lanes with 4.0-foot shoulders and standard concrete railing. The clear roadway width and out to out width will be 30.0 feet and 33.0 feet, respectively, and will be approximately 63.75 feet in length. Approach work is expected to include milling and hot mix asphalt (HMA) resurfacing on either side of the bridge. The roadway section will include two 11-foot lanes with 4.0-foot paved shoulders. New guardrail will be constructed along both approaches to the bridge. Riprap drainage turnouts will be constructed in all four quadrants. Riprap will be added along the bridge embankments and both banks of Phillips Ditch for erosion control. The length of the project is approximately 710 feet along CR 250 W.

This project will require approximately 0.66 acre of new permanent right-of-way (ROW) and 0.02 acre of temporary ROW. Tree clearing totaling approximately 0.15 acre will occur. Suitable summer bat habitat is present within and adjacent to the project area. Tree clearing will take place within 100 feet of a roadway and will occur during the inactive season. The dominant tree species is eastern cottonwood (*Populus deltoides*). Mitigation for removed suitable summer bat habitat is not anticipated, but mitigation may be required by the Indiana Department of Natural Resources for trees removed within the Phillips Ditch floodway. No new permanent lighting will be installed for this project. Use of temporary lighting may be necessary if night work is required. Construction is anticipated to begin in summer of 2026.

A review of the USFWS database completed May 16, 2022 by INDOT LaPorte District did not indicate the presence of endangered bat species or their hibernacula within 0.5 mile of the project area. An inspection of the structure by Lochmueller Group staff occurred on August 2, 2022 and there was no evidence of bats.

Project Location:

The approximate location of the project can be viewed in Google Maps: <https://www.google.com/maps/@41.2951677,-87.1126795443352,14z>



Counties: Porter County, Indiana

ENDANGERED SPECIES ACT SPECIES

There is a total of 5 threatened, endangered, or candidate species on this species list.

Species on this list should be considered in an effects analysis for your project and could include species that exist in another geographic area. For example, certain fish may appear on the species list because a project could affect downstream species.

IPaC does not display listed species or critical habitats under the sole jurisdiction of NOAA Fisheries¹, as USFWS does not have the authority to speak on behalf of NOAA and the Department of Commerce.

See the "Critical habitats" section below for those critical habitats that lie wholly or partially within your project area under this office's jurisdiction. Please contact the designated FWS office if you have questions.

1. [NOAA Fisheries](#), also known as the National Marine Fisheries Service (NMFS), is an office of the National Oceanic and Atmospheric Administration within the Department of Commerce.

MAMMALS

NAME	STATUS
Indiana Bat <i>Myotis sodalis</i> There is final critical habitat for this species. Your location does not overlap the critical habitat. Species profile: https://ecos.fws.gov/ecp/species/5949	Endangered
Northern Long-eared Bat <i>Myotis septentrionalis</i> No critical habitat has been designated for this species. Species profile: https://ecos.fws.gov/ecp/species/9045	Endangered
Tricolored Bat <i>Perimyotis subflavus</i> No critical habitat has been designated for this species. Species profile: https://ecos.fws.gov/ecp/species/10515	Proposed Endangered

BIRDS

NAME	STATUS
Whooping Crane <i>Grus americana</i> Population: U.S.A. (AL, AR, CO, FL, GA, ID, IL, IN, IA, KY, LA, MI, MN, MS, MO, NC, NM, OH, SC, TN, UT, VA, WI, WV, western half of WY) No critical habitat has been designated for this species. Species profile: https://ecos.fws.gov/ecp/species/758	Experimental Population, Non- Essential

INSECTS

NAME	STATUS
Monarch Butterfly <i>Danaus plexippus</i> No critical habitat has been designated for this species. Species profile: https://ecos.fws.gov/ecp/species/9743	Candidate

CRITICAL HABITATS

THERE ARE NO CRITICAL HABITATS WITHIN YOUR PROJECT AREA UNDER THIS OFFICE'S JURISDICTION.

YOU ARE STILL REQUIRED TO DETERMINE IF YOUR PROJECT(S) MAY HAVE EFFECTS ON ALL ABOVE LISTED SPECIES.

MIGRATORY BIRDS

Certain birds are protected under the Migratory Bird Treaty Act¹ and the Bald and Golden Eagle Protection Act².

Any person or organization who plans or conducts activities that may result in impacts to migratory birds, eagles, and their habitats should follow appropriate regulations and consider implementing appropriate conservation measures, as described [below](#).

-
1. The [Migratory Birds Treaty Act](#) of 1918.
 2. The [Bald and Golden Eagle Protection Act](#) of 1940.
 3. 50 C.F.R. Sec. 10.12 and 16 U.S.C. Sec. 668(a)

The birds listed below are birds of particular concern either because they occur on the [USFWS Birds of Conservation Concern \(BCC\) list](#) or warrant special attention in your project location. To learn more about the levels of concern for birds on your list and how this list is generated, see the FAQ [below](#). This is not a list of every bird you may find in this location, nor a guarantee that every bird on this list will be found in your project area. To see exact locations of where birders and the general public have sighted birds in and around your project area, visit the [E-bird data mapping tool](#) (Tip: enter your location, desired date range and a species on your list). For projects that occur off the Atlantic Coast, additional maps and models detailing the relative occurrence and abundance of bird species on your list are available. Links to additional information about Atlantic Coast birds, and other important information about your migratory bird list, including how to properly interpret and use your migratory bird report, can be found [below](#).

For guidance on when to schedule activities or implement avoidance and minimization measures to reduce impacts to migratory birds on your list, click on the PROBABILITY OF PRESENCE SUMMARY at the top of your list to see when these birds are most likely to be present and breeding in your project area.

NAME	BREEDING SEASON
Bald Eagle <i>Haliaeetus leucocephalus</i> This is not a Bird of Conservation Concern (BCC) in this area, but warrants attention because of the Eagle Act or for potential susceptibilities in offshore areas from certain types of development or activities.	Breeds Oct 15 to Aug 31
Black-billed Cuckoo <i>Coccyzus erythrophthalmus</i> This is a Bird of Conservation Concern (BCC) throughout its range in the continental USA and Alaska. https://ecos.fws.gov/ecp/species/9399	Breeds May 15 to Oct 10

NAME	BREEDING SEASON
Cerulean Warbler <i>Dendroica cerulea</i> This is a Bird of Conservation Concern (BCC) throughout its range in the continental USA and Alaska. https://ecos.fws.gov/ecp/species/2974	Breeds Apr 21 to Jul 20
Chimney Swift <i>Chaetura pelagica</i> This is a Bird of Conservation Concern (BCC) throughout its range in the continental USA and Alaska.	Breeds Mar 15 to Aug 25
Red-headed Woodpecker <i>Melanerpes erythrocephalus</i> This is a Bird of Conservation Concern (BCC) throughout its range in the continental USA and Alaska.	Breeds May 10 to Sep 10

PROBABILITY OF PRESENCE SUMMARY

The graphs below provide our best understanding of when birds of concern are most likely to be present in your project area. This information can be used to tailor and schedule your project activities to avoid or minimize impacts to birds. Please make sure you read and understand the FAQ "Proper Interpretation and Use of Your Migratory Bird Report" before using or attempting to interpret this report.

Probability of Presence (■)

Each green bar represents the bird's relative probability of presence in the 10km grid cell(s) your project overlaps during a particular week of the year. (A year is represented as 12 4-week months.) A taller bar indicates a higher probability of species presence. The survey effort (see below) can be used to establish a level of confidence in the presence score. One can have higher confidence in the presence score if the corresponding survey effort is also high.

How is the probability of presence score calculated? The calculation is done in three steps:

1. The probability of presence for each week is calculated as the number of survey events in the week where the species was detected divided by the total number of survey events for that week. For example, if in week 12 there were 20 survey events and the Spotted Towhee was found in 5 of them, the probability of presence of the Spotted Towhee in week 12 is 0.25.
2. To properly present the pattern of presence across the year, the relative probability of presence is calculated. This is the probability of presence divided by the maximum probability of presence across all weeks. For example, imagine the probability of presence in week 20 for the Spotted Towhee is 0.05, and that the probability of presence at week 12 (0.25) is the maximum of any week of the year. The relative probability of presence on week 12 is $0.25/0.25 = 1$; at week 20 it is $0.05/0.25 = 0.2$.
3. The relative probability of presence calculated in the previous step undergoes a statistical conversion so that all possible values fall between 0 and 10, inclusive. This is the probability of presence score.

Breeding Season (■)

Yellow bars denote a very liberal estimate of the time-frame inside which the bird breeds across its entire range. If there are no yellow bars shown for a bird, it does not breed in your project area.

Survey Effort (|)

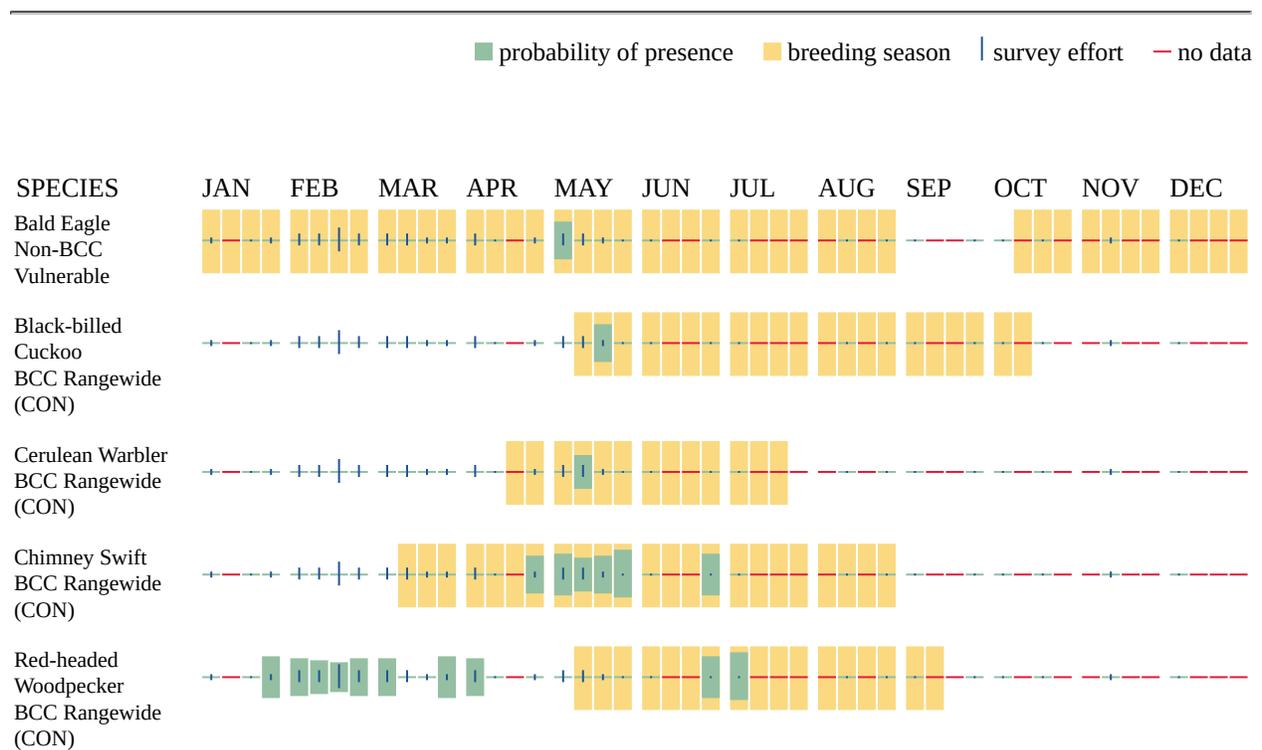
Vertical black lines superimposed on probability of presence bars indicate the number of surveys performed for that species in the 10km grid cell(s) your project area overlaps. The number of surveys is expressed as a range, for example, 33 to 64 surveys.

No Data (-)

A week is marked as having no data if there were no survey events for that week.

Survey Timeframe

Surveys from only the last 10 years are used in order to ensure delivery of currently relevant information. The exception to this is areas off the Atlantic coast, where bird returns are based on all years of available data, since data in these areas is currently much more sparse.



Additional information can be found using the following links:

- Birds of Conservation Concern <https://www.fws.gov/program/migratory-birds/species>
- Measures for avoiding and minimizing impacts to birds <https://www.fws.gov/library/collections/avoiding-and-minimizing-incident-take-migratory-birds>
- Nationwide conservation measures for birds <https://www.fws.gov/sites/default/files/documents/nationwide-standard-conservation-measures.pdf>

MIGRATORY BIRDS FAQ

Tell me more about conservation measures I can implement to avoid or minimize impacts to migratory birds.

[Nationwide Conservation Measures](#) describes measures that can help avoid and minimize impacts to all birds at any location year round. Implementation of these measures is particularly important when birds are most likely to occur in the project area. When birds may be breeding in the area, identifying the locations of any active nests and avoiding their destruction is a very helpful impact minimization measure. To see when birds are most likely to occur and be breeding in your project area, view the Probability of Presence Summary. [Additional measures](#) or [permits](#) may be advisable depending on the type of activity you are conducting and the type of infrastructure or bird species present on your project site.

What does IPaC use to generate the list of migratory birds that potentially occur in my specified location?

The Migratory Bird Resource List is comprised of USFWS [Birds of Conservation Concern \(BCC\)](#) and other species that may warrant special attention in your project location.

The migratory bird list generated for your project is derived from data provided by the [Avian Knowledge Network \(AKN\)](#). The AKN data is based on a growing collection of [survey, banding, and citizen science datasets](#) and is queried and filtered to return a list of those birds reported as occurring in the 10km grid cell(s) which your project intersects, and that have been identified as warranting special attention because they are a BCC species in that area, an eagle ([Eagle Act](#) requirements may apply), or a species that has a particular vulnerability to offshore activities or development.

Again, the Migratory Bird Resource list includes only a subset of birds that may occur in your project area. It is not representative of all birds that may occur in your project area. To get a list of all birds potentially present in your project area, please visit the [Rapid Avian Information Locator \(RAIL\) Tool](#).

What does IPaC use to generate the probability of presence graphs for the migratory birds potentially occurring in my specified location?

The probability of presence graphs associated with your migratory bird list are based on data provided by the [Avian Knowledge Network \(AKN\)](#). This data is derived from a growing collection of [survey, banding, and citizen science datasets](#).

Probability of presence data is continuously being updated as new and better information becomes available. To learn more about how the probability of presence graphs are produced and how to interpret them, go the Probability of Presence Summary and then click on the "Tell me about these graphs" link.

How do I know if a bird is breeding, wintering or migrating in my area?

To see what part of a particular bird's range your project area falls within (i.e. breeding, wintering, migrating or year-round), you may query your location using the [RAIL Tool](#) and look at the range maps provided for birds in your area at the bottom of the profiles provided for each bird in your results. If a bird on your migratory bird species list has a breeding season associated with it, if that bird does occur in your project area, there may be nests present at some point

within the timeframe specified. If "Breeds elsewhere" is indicated, then the bird likely does not breed in your project area.

What are the levels of concern for migratory birds?

Migratory birds delivered through IPaC fall into the following distinct categories of concern:

1. "BCC Rangewide" birds are [Birds of Conservation Concern](#) (BCC) that are of concern throughout their range anywhere within the USA (including Hawaii, the Pacific Islands, Puerto Rico, and the Virgin Islands);
2. "BCC - BCR" birds are BCCs that are of concern only in particular Bird Conservation Regions (BCRs) in the continental USA; and
3. "Non-BCC - Vulnerable" birds are not BCC species in your project area, but appear on your list either because of the [Eagle Act](#) requirements (for eagles) or (for non-eagles) potential susceptibilities in offshore areas from certain types of development or activities (e.g. offshore energy development or longline fishing).

Although it is important to try to avoid and minimize impacts to all birds, efforts should be made, in particular, to avoid and minimize impacts to the birds on this list, especially eagles and BCC species of rangewide concern. For more information on conservation measures you can implement to help avoid and minimize migratory bird impacts and requirements for eagles, please see the FAQs for these topics.

Details about birds that are potentially affected by offshore projects

For additional details about the relative occurrence and abundance of both individual bird species and groups of bird species within your project area off the Atlantic Coast, please visit the [Northeast Ocean Data Portal](#). The Portal also offers data and information about other taxa besides birds that may be helpful to you in your project review. Alternately, you may download the bird model results files underlying the portal maps through the [NOAA NCCOS Integrative Statistical Modeling and Predictive Mapping of Marine Bird Distributions and Abundance on the Atlantic Outer Continental Shelf](#) project webpage.

Bird tracking data can also provide additional details about occurrence and habitat use throughout the year, including migration. Models relying on survey data may not include this information. For additional information on marine bird tracking data, see the [Diving Bird Study](#) and the [nanotag studies](#) or contact [Caleb Spiegel](#) or [Pam Loring](#).

What if I have eagles on my list?

If your project has the potential to disturb or kill eagles, you may need to [obtain a permit](#) to avoid violating the Eagle Act should such impacts occur.

Proper Interpretation and Use of Your Migratory Bird Report

The migratory bird list generated is not a list of all birds in your project area, only a subset of birds of priority concern. To learn more about how your list is generated, and see options for identifying what other birds may be in your project area, please see the FAQ "What does IPaC use to generate the migratory birds potentially occurring in my specified location". Please be aware this report provides the "probability of presence" of birds within the 10 km grid cell(s) that overlap your project; not your exact project footprint. On the graphs provided, please also look carefully at the survey effort (indicated by the black vertical bar) and for the existence of the "no

data" indicator (a red horizontal bar). A high survey effort is the key component. If the survey effort is high, then the probability of presence score can be viewed as more dependable. In contrast, a low survey effort bar or no data bar means a lack of data and, therefore, a lack of certainty about presence of the species. This list is not perfect; it is simply a starting point for identifying what birds of concern have the potential to be in your project area, when they might be there, and if they might be breeding (which means nests might be present). The list helps you know what to look for to confirm presence, and helps guide you in knowing when to implement conservation measures to avoid or minimize potential impacts from your project activities, should presence be confirmed. To learn more about conservation measures, visit the FAQ "Tell me about conservation measures I can implement to avoid or minimize impacts to migratory birds" at the bottom of your migratory bird trust resources page.

WETLANDS

Impacts to [NWI wetlands](#) and other aquatic habitats may be subject to regulation under Section 404 of the Clean Water Act, or other State/Federal statutes.

For more information please contact the Regulatory Program of the local [U.S. Army Corps of Engineers District](#).

Please note that the NWI data being shown may be out of date. We are currently working to update our NWI data set. We recommend you verify these results with a site visit to determine the actual extent of wetlands on site.

RIVERINE

- [R2UBEx](#)

IPAC USER CONTACT INFORMATION

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LEAD AGENCY CONTACT INFORMATION

Lead Agency: Federal Highway Administration



United States Department of the Interior



FISH AND WILDLIFE SERVICE
Indiana Ecological Services Field Office
620 South Walker Street
Bloomington, IN 47403-2121
Phone: (812) 334-4261 Fax: (812) 334-4273

In Reply Refer To:

December 21, 2022

Project code: 2023-0026602

Project Name: Des. No. 2003041, CR 250 W over Phillips Ditch, Bridge Project, Porter County, Indiana

Subject: Concurrence verification letter for the 'Des. No. 2003041, CR 250 W over Phillips Ditch, Bridge Project, Porter County, Indiana' project under the revised February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects within the Range of the Indiana Bat and Northern Long-eared Bat.

To whom it may concern:

The U.S. Fish and Wildlife Service (Service) has received your request dated December 21, 2022 to verify that the **Des. No. 2003041, CR 250 W over Phillips Ditch, Bridge Project, Porter County, Indiana** (Proposed Action) may rely on the concurrence provided in the February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects within the Range of the Indiana Bat and Northern Long-eared Bat (PBO) to satisfy requirements under Section 7(a)(2) of the Endangered Species Act of 1973 (ESA) (87 Stat. 884, as amended; 16 U.S.C 1531 *et seq.*).

Based on the information you provided (Project Description shown below), you have determined that the Proposed Action is within the scope and adheres to the criteria of the PBO, including the adoption of applicable avoidance and minimization measures, and may affect, but is not likely to adversely affect (NLAA) the endangered Indiana bat (*Myotis sodalis*) and/or the threatened Northern long-eared bat (*Myotis septentrionalis*). Consultation with the Service pursuant to Section 7(a)(2) of the Endangered Species Act of 1973 (ESA) (87 Stat. 884, as amended; 16 U.S.C. 1531 *et seq.*) is required.

The Service has 14 calendar days to notify the lead Federal action agency or designated non-federal representative if we determine that the Proposed Action does not meet the criteria for a NLAA determination under the PBO. If we do not notify the lead Federal action agency or designated non-federal representative within that timeframe, you may proceed with the Proposed Action under the terms of the NLAA concurrence provided in the PBO. This verification period allows Service Field Offices to apply local knowledge to implementation of the PBO, as we may identify a small subset of actions having impacts that were unanticipated. In such instances,

Service Field Offices may request additional information that is necessary to verify inclusion of the proposed action under the PBO.

NOTE: The Service reclassified the NLEB as an endangered species on November 30, 2022. This ruling becomes effective on January 30, 2023. This NLAA determination does not require reinitiation. For projects requiring consultation after the effective date of January 30, 2023, please use the 2022 FHWA, FRA, FTA PBO.

For Proposed Actions that include bridge/culvert or structure removal, replacement, and/or maintenance activities: If your initial bridge/culvert or structure assessment documented signs of bat use or occupancy, or an assessment failed to detect Indiana bats and/or NLEBs, yet are later detected prior to, or during construction, please submit the Post Assessment Discovery of Bats at Bridge/Culvert or Structure Form (User Guide Appendix E) to this Service Office within 2 working days of any potential take. In these instances, potential incidental take of Indiana bats and/or NLEBs is covered under the Incidental Take Statement in the 2018 FHWA, FRA, FTA PBO (provided that the take is reported to the Service).

If the Proposed Action is modified, or new information reveals that it may affect the Indiana bat and/or Northern long-eared bat in a manner or to an extent not considered in the PBO, further review to conclude the requirements of ESA Section 7(a)(2) may be required. If the Proposed Action may affect any other federally-listed or proposed species, and/or any designated critical habitat, additional consultation between the lead Federal action agency and this Service Office is required. If the proposed action has the potential to take bald or golden eagles, additional coordination with the Service under the Bald and Golden Eagle Protection Act may also be required. In either of these circumstances, please contact this Service Office.

The following species may occur in your project area and **are not** covered by this determination:

- Monarch Butterfly *Danaus plexippus* Candidate

Project Description

The following project name and description was collected in IPaC as part of the endangered species review process.

Name

Des. No. 2003041, CR 250 W over Phillips Ditch, Bridge Project, Porter County, Indiana

Description

Porter County, with funding from the Federal Highway Administration (FHWA) and oversight from the Indiana Department of Transportation (INDOT), intends to proceed with the following bridge project along County Road (CR) 250 W approximately 1.5 miles south of State Road 8 in LaPorte County, Indiana.

The proposed project involves the replacement of the existing bridge, Bridge No. 64-00066. The replacement structure is expected to be a single-span prestressed concrete box beam bridge featuring two 11-foot wide travel lanes with 4.0-foot shoulders and standard concrete railing. The clear roadway width and out to out width will be 30.0 feet and 33.0 feet, respectively, and will be approximately 63.75 feet in length. Approach work is expected to include milling and hot mix asphalt (HMA) resurfacing on either side of the bridge. The roadway section will include two 11-foot lanes with 4.0-foot paved shoulders. New guardrail will be constructed along both approaches to the bridge. Riprap drainage turnouts will be constructed in all four quadrants. Riprap will be added along the bridge embankments and both banks of Phillips Ditch for erosion control. The length of the project is approximately 710 feet along CR 250 W.

This project will require approximately 0.65 acre of new permanent right-of-way (ROW) and 0.02 acre of temporary ROW. Tree clearing totaling approximately 0.15 acre will occur. Suitable summer bat habitat is present within and adjacent to the project area. Tree clearing will take place within 100 feet of a roadway and will occur during the inactive season. The dominant tree species is eastern cottonwood (*Populus deltoides*). Mitigation for removed suitable summer bat habitat is not anticipated, but mitigation may be required by the Indiana Department of Natural Resources for trees removed within the Phillips Ditch floodway. No new permanent lighting will be installed for this project. Use of temporary lighting may be necessary if night work is required. Construction is anticipated to begin in summer of 2026.

A review of the USFWS database completed May 16, 2022 by INDOT LaPorte District did not indicate the presence of endangered bat species or their hibernacula within 0.5 mile of the project area. An inspection of the structure by Lochmueller Group staff occurred on August 2, 2022 and there was no evidence of bats.

Determination Key Result

Based on your answers provided, this project(s) may affect, but is not likely to adversely affect the endangered Indiana bat and/or the threatened Northern long-eared bat, therefore, consultation with the U.S. Fish and Wildlife Service pursuant to Section 7(a)(2) of the Endangered Species Act of 1973 (ESA) (87 Stat. 884, as amended 16 U.S.C. 1531 *et seq.*) is required. However, also based on your answers provided, this project may rely on the concurrence provided in the revised February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects within the Range of the Indiana Bat and Northern Long-eared Bat.

Qualification Interview

1. Is the project within the range of the Indiana bat^[1]?

[1] See [Indiana bat species profile](#)

Automatically answered

Yes

2. Is the project within the range of the Northern long-eared bat^[1]?

[1] See [Northern long-eared bat species profile](#)

Automatically answered

Yes

3. Which Federal Agency is the lead for the action?

A) *Federal Highway Administration (FHWA)*

4. Are *all* project activities limited to non-construction^[1] activities only? (examples of non-construction activities include: bridge/abandoned structure assessments, surveys, planning and technical studies, property inspections, and property sales)

[1] Construction refers to activities involving ground disturbance, percussive noise, and/or lighting.

No

5. Does the project include *any* activities that are **greater than** 300 feet from existing road/rail surfaces^[1]?

[1] Road surface is defined as the actively used [e.g. motorized vehicles] driving surface and shoulders [may be pavement, gravel, etc.] and rail surface is defined as the edge of the actively used rail ballast.

No

6. Does the project include *any* activities **within** 0.5 miles of a known Indiana bat and/or NLEB hibernaculum^[1]?

[1] For the purpose of this consultation, a hibernaculum is a site, most often a cave or mine, where bats hibernate during the winter (see suitable habitat), but could also include bridges and structures if bats are found to be hibernating there during the winter.

No

7. Is the project located **within** a karst area?

No

8. Is there *any* suitable^[1] summer habitat for Indiana Bat or NLEB **within** the project action area^[2]? (includes any trees suitable for maternity, roosting, foraging, or travelling habitat)

[1] See the Service's [summer survey guidance](#) for our current definitions of suitable habitat.

[2] The action area is defined as all areas to be affected directly or indirectly by the Federal action and not merely the immediate area involved in the action (50 CFR Section 402.02). Further clarification is provided by the [User's Guide for the Range-wide Programmatic Consultation for Indiana Bat and Northern Long-eared Bat](#).

Yes

9. Will the project remove *any* suitable summer habitat^[1] and/or remove/trim any existing trees **within** suitable summer habitat?

[1] See the Service's [summer survey guidance](#) for our current definitions of suitable habitat.

Yes

10. Will the project clear more than 20 acres of suitable habitat per 5-mile section of road/rail?

No

11. Have presence/probable absence (P/A) summer surveys^{[1][2]} been conducted^{[3][4]} **within** the suitable habitat located within your project action area?

[1] See the Service's [summer survey guidance](#) for our current definitions of suitable habitat.

[2] Presence/probable absence summer surveys conducted within the fall swarming/spring emergence home range of a documented Indiana bat hibernaculum (contact local Service Field Office for appropriate distance from hibernacula) that result in a negative finding requires additional consultation with the local Service Field Office to determine if clearing of forested habitat is appropriate and/or if seasonal clearing restrictions are needed to avoid and minimize potential adverse effects on fall swarming and spring emerging Indiana bats.

[3] For projects within the range of either the Indiana bat or NLEB in which suitable habitat is present, and no bat surveys have been conducted, the transportation agency will assume presence of the appropriate species. This assumption of presence should be based upon the presence of suitable habitat and the capability of bats to occupy it because of their mobility.

[4] Negative presence/probable absence survey results obtained using the [summer survey guidance](#) are valid for a minimum of two years from the completion of the survey unless new information (e.g., other nearby surveys) suggest otherwise.

No

12. Does the project include activities **within documented Indiana bat habitat**^{[1][2]}?

[1] Documented roosting or foraging habitat – for the purposes of this consultation, we are considering documented habitat as that where Indiana bats and/or NLEB have actually been captured and tracked using (1) radio telemetry to roosts; (2) radio telemetry triangulation/triangulation to estimate foraging areas; or (3) foraging areas with repeated use documented using acoustics. Documented roosting habitat is also considered as suitable summer habitat within 0.25 miles of documented roosts.)

[2] For the purposes of this key, we are considering documented corridors as that where Indiana bats and/or NLEB have actually been captured and tracked to using (1) radio telemetry; or (2) treed corridors located directly between documented roosting and foraging habitat.

No

13. Will the removal or trimming of habitat or trees occur **within** suitable but **undocumented Indiana bat** roosting/foraging habitat or travel corridors?

Yes

14. What time of year will the removal or trimming of habitat or trees **within** suitable but **undocumented Indiana bat** roosting/foraging habitat or travel corridors occur^[1]?

[1] Coordinate with the local Service Field Office for appropriate dates.

B) During the inactive season

15. Does the project include activities **within documented NLEB habitat**^{[1][2]}?

[1] Documented roosting or foraging habitat – for the purposes of this consultation, we are considering documented habitat as that where Indiana bats and/or NLEB have actually been captured and tracked using (1) radio telemetry to roosts; (2) radio telemetry triangulation/triangulation to estimate foraging areas; or (3) foraging areas with repeated use documented using acoustics. Documented roosting habitat is also considered as suitable summer habitat within 0.25 miles of documented roosts.)

[2] For the purposes of this key, we are considering documented corridors as that where Indiana bats and/or NLEB have actually been captured and tracked to using (1) radio telemetry; or (2) treed corridors located directly between documented roosting and foraging habitat.

No

16. Will the removal or trimming of habitat or trees occur **within** suitable but **undocumented NLEB** roosting/foraging habitat or travel corridors?

Yes

17. What time of year will the removal or trimming of habitat or trees **within** suitable but **undocumented NLEB** roosting/foraging habitat or travel corridors occur?

B) During the inactive season

18. Will *any* tree trimming or removal occur **within** 100 feet of existing road/rail surfaces?

Yes

19. Will *any* tree trimming or removal occur **between** 100-300 feet of existing road/rail surfaces?

No

20. Are *all* trees that are being removed clearly demarcated?
Yes
21. Will the removal of habitat or the removal/trimming of trees include installing new or replacing existing **permanent** lighting?
No
22. Does the project include wetland or stream protection activities associated with compensatory wetland mitigation?
No
23. Does the project include slash pile burning?
No
24. Does the project include *any* bridge removal, replacement, and/or maintenance activities (e.g., any bridge repair, retrofit, maintenance, and/or rehabilitation work)?
Yes
25. Is there *any* suitable habitat^[1] for Indiana bat or NLEB **within** 1,000 feet of the bridge? (includes any trees suitable for maternity, roosting, foraging, or travelling habitat)

[1] See the Service's current [summer survey guidance](#) for our current definitions of suitable habitat.

Yes

26. Has a bridge assessment^[1] been conducted **within** the last 24 months^[2] to determine if the bridge is being used by bats?

[1] See [User Guide Appendix D](#) for bridge/structure assessment guidance

[2] Assessments must be completed no more than 2 years prior to conducting any work below the deck surface on all bridges that meet the physical characteristics described in the Programmatic Consultation, regardless of whether assessments have been conducted in the past. Due to the transitory nature of bat use, a negative result in one year does not guarantee that bats will not use that bridge/structure in subsequent years.

Yes

SUBMITTED DOCUMENTS

- *signed_20220802_Bridge Culvert Bat Assessment Form_Apr 2020.pdf* <https://ipac.ecosphere.fws.gov/project/VC2D7ABQENAHHRHFMM5YQQSPMWE/projectDocuments/120422081>

27. Did the bridge assessment detect *any* signs of Indiana bats and/or NLEBs roosting in/under the bridge (bats, guano, etc.)^[1]?

[1] If bridge assessment detects signs of *any* species of bats, coordination with the local FWS office is needed to identify potential threatened or endangered bat species. Additional studies may be undertaken to try to identify which bat species may be utilizing the bridge prior to allowing *any* work to proceed.

Note: There is a small chance bridge assessments for bat occupancy do not detect bats. Should a small number of bats be observed roosting on a bridge just prior to or during construction, such that take is likely to occur or does occur in the form of harassment, injury or death, the PBO requires the action agency to report the take. Report all unanticipated take within 2 working days of the incident to the USFWS. Construction activities may continue without delay provided the take is reported to the USFWS and is limited to 5 bats per project.

No

28. Will the bridge removal, replacement, and/or maintenance activities include installing new or replacing existing **permanent** lighting?

No

29. Does the project include the removal, replacement, and/or maintenance of *any* structure other than a bridge? (e.g., rest areas, offices, sheds, outbuildings, barns, parking garages, etc.)

No

30. Will the project involve the use of **temporary** lighting *during* the active season?

Yes

31. Is there *any* suitable habitat **within** 1,000 feet of the location(s) where **temporary** lighting will be used?

Yes

32. Will the project install new or replace existing **permanent** lighting?

No

33. Does the project include percussives or other activities (**not including tree removal/trimming or bridge/structure work**) that will increase noise levels above existing traffic/background levels?

No

34. Are *all* project activities that are **not associated with** habitat removal, tree removal/trimming, bridge and/or structure activities, temporary or permanent lighting, or use of percussives, limited to actions that DO NOT cause any additional stressors to the bat species?

Examples: lining roadways, unlighted signage, rail road crossing signals, signal lighting, and minor road repair such as asphalt fill of potholes, etc.

Yes

35. Will the project raise the road profile **above the tree canopy**?

No

36. Are the project activities that are not associated with habitat removal, tree removal/trimming, bridge and/or structure activities, temporary or permanent lighting, or use of percussives consistent with a No Effect determination in this key?

Automatically answered

Yes, other project activities are limited to actions that DO NOT cause any additional stressors to the bat species as described in the BA/BO

37. Is the habitat removal portion of this project consistent with a Not Likely to Adversely Affect determination in this key?

Automatically answered

Yes, because the tree removal/trimming that occurs outside of the Indiana bat's active season occurs greater than 0.5 miles from the nearest hibernaculum, is less than 100 feet from the existing road/rail surface, includes clear demarcation of the trees that are to be removed, and does not alter documented roosts and/or surrounding summer habitat within 0.25 miles of a documented roost.

38. Is the habitat removal portion of this project consistent with a Not Likely to Adversely Affect determination in this key?

Automatically answered

Yes, because the tree removal/trimming that occurs outside of the NLEB's active season occurs greater than 0.5 miles from the nearest hibernaculum, is less than 100 feet from the existing road/rail surface, includes clear demarcation of the trees that are to be removed, and does not alter documented roosts and/or surrounding summer habitat within 0.25 miles of a documented roost.

39. Is the bridge removal, replacement, or maintenance activities portion of this project consistent with a No Effect determination in this key?

Automatically answered

Yes, because the bridge has been assessed using the criteria documented in the BA and no signs of bats were detected

40. **General AMM 1**

Will the project ensure *all* operators, employees, and contractors working in areas of known or presumed bat habitat are aware of *all* FHWA/FRA/FTA (Transportation Agencies) environmental commitments, including all applicable Avoidance and Minimization Measures?

Yes

41. **Tree Removal AMM 1**

Can *all* phases/aspects of the project (e.g., temporary work areas, alignments) be modified, to the extent practicable, to avoid tree removal^[1] in excess of what is required to implement the project safely?

Note: Tree Removal AMM 1 is a minimization measure, the full implementation of which may not always be practicable. Projects may still be NLAA as long as Tree Removal AMMs 2, 3, and 4 are implemented and LAA as long as Tree Removal AMMs 3, 5, 6, and 7 are implemented.

[1] The word “trees” as used in the AMMs refers to trees that are suitable habitat for each species within their range. See the USFWS’ current summer survey guidance for our latest definitions of suitable habitat.

Yes

42. **Tree Removal AMM 3**

Can tree removal be limited to that specified in project plans and ensure that contractors understand clearing limits and how they are marked in the field (e.g., install bright colored flagging/fencing prior to any tree clearing to ensure contractors stay within clearing limits)?

Yes

43. **Tree Removal AMM 4**

Can the project avoid cutting down/removal of *all* (1) **documented**^[1] Indiana bat or NLEB roosts^[2] (that are still suitable for roosting), (2) trees **within** 0.25 miles of roosts, and (3) documented foraging habitat any time of year?

[1] The word documented means habitat where bats have actually been captured and/or tracked.

[2] Documented roosting or foraging habitat – for the purposes of this consultation, we are considering documented habitat as that where Indiana bats and/or NLEB have actually been captured and tracked using (1) radio telemetry to roosts; (2) radio telemetry biangulation/triangulation to estimate foraging areas; or (3) foraging areas with repeated use documented using acoustics. Documented roosting habitat is also considered as suitable summer habitat within 0.25 miles of documented roosts.)

Yes

44. **Lighting AMM 1**

Will *all* **temporary** lighting be directed away from suitable habitat during the active season?

Yes

Project Questionnaire

1. Have you made a No Effect determination for *all* other species indicated on the FWS IPaC generated species list?

N/A

2. Have you made a May Affect determination for *any* other species on the FWS IPaC generated species list?

N/A

3. How many acres^[1] of trees are proposed for removal between 0-100 feet of the existing road/rail surface?

[1] If described as number of trees, multiply by 0.09 to convert to acreage and enter that number.

0.15

4. Please describe the proposed bridge work:

The project involves removal of the existing bridge and replacement with a single-span prestressed concrete box beam bridge. Approach work is expected to include milling and hot mix asphalt (HMA) resurfacing on either side of the bridge. Riprap drainage turnouts will be constructed in all four quadrants. New guardrail will be constructed along both approaches to the bridge.

5. Please state the timing of all proposed bridge work:

Construction will occur in spring of 2026

6. Please enter the date of the bridge assessment:

August 2, 2022

Avoidance And Minimization Measures (AMMs)

This determination key result includes the commitment to implement the following Avoidance and Minimization Measures (AMMs):

TREE REMOVAL AMM 4

Do not remove **documented** Indiana bat or NLEB roosts that are still suitable for roosting, or trees within 0.25 miles of roosts, or **documented** foraging habitat any time of year.

GENERAL AMM 1

Ensure all operators, employees, and contractors working in areas of known or presumed bat habitat are aware of all FHWA/FRA/FTA (Transportation Agencies) environmental commitments, including all applicable AMMs.

TREE REMOVAL AMM 1

Modify all phases/aspects of the project (e.g., temporary work areas, alignments) to avoid tree removal.

LIGHTING AMM 1

Direct temporary lighting away from suitable habitat during the active season.

TREE REMOVAL AMM 2

Apply time of year restrictions for tree removal when bats are not likely to be present, or limit tree removal to 10 or fewer trees per project at any time of year within 100 feet of existing road/rail surface and **outside of documented** roosting/foraging habitat or travel corridors; visual emergence survey must be conducted with no bats observed.

TREE REMOVAL AMM 3

Ensure tree removal is limited to that specified in project plans and ensure that contractors understand clearing limits and how they are marked in the field (e.g., install bright colored flagging/fencing prior to any tree clearing to ensure contractors stay within clearing limits).

Determination Key Description: FHWA, FRA, FTA Programmatic Consultation For Transportation Projects Affecting NLEB Or Indiana Bat

This key was last updated in IPaC on December 01, 2022. Keys are subject to periodic revision.

This decision key is intended for projects/activities funded or authorized by the Federal Highway Administration (FHWA), Federal Railroad Administration (FRA), and/or Federal Transit Administration (FTA), which may require consultation with the U.S. Fish and Wildlife Service (Service) under Section 7 of the Endangered Species Act (ESA) for the endangered **Indiana bat** (*Myotis sodalis*) and the threatened **Northern long-eared bat** (NLEB) (*Myotis septentrionalis*).

This decision key should only be used to verify project applicability with the Service's [February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects](#). The programmatic biological opinion covers limited transportation activities that may affect either bat species, and addresses situations that are both likely and not likely to adversely affect either bat species. This decision key will assist in identifying the effect of a specific project/activity and applicability of the programmatic consultation. The programmatic biological opinion is not intended to cover all types of transportation actions. Activities outside the scope of the programmatic biological opinion, or that may affect ESA-listed species other than the Indiana bat or NLEB, or any designated critical habitat, may require additional ESA Section 7 consultation.

IPaC User Contact Information

Agency: Indiana Department of Transportation
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Email: smichels@indot.in.gov
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Lead Agency Contact Information

Lead Agency: Federal Highway Administration

Bridge/Structure Bat Assessment Form

Date & Time of Assessment 08/02/2022 1:00 PM	DOT Project Number Des. No. 2003041	Route/Facility Carried CR 250 W over Phillips Ditch	County Porter
Federal Structure ID Str. No. 64-00001 NBI: 6400001	Structure Coordinates 41.43431 (latitude and longitude) -86.961983	Structure Height (approximate) 10 ft	Structure Length 52.5 feet
Structure Type (check one)		Structure Material (check all that apply)	
Bridge Construction Style		Deck Material	Beam Material
<input type="radio"/> Cast-in-place 	<input type="radio"/> Pre-stressed Girder 	<input type="checkbox"/> Metal	<input type="checkbox"/> None
<input type="radio"/> Flat Slab/Box 	<input type="radio"/> Steel I-beam 	<input checked="" type="checkbox"/> Concrete	<input checked="" type="checkbox"/> Concrete
<input type="radio"/> Truss 	<input type="radio"/> Covered 	<input type="checkbox"/> Timber	<input type="checkbox"/> Steel
<input checked="" type="radio"/> Parallel Box Beam 	<input type="radio"/> Other:	<input type="checkbox"/> Open grid	<input type="checkbox"/> Timber
		<input type="checkbox"/> Other:	<input type="checkbox"/> Other:
Culvert Type	Other Structure	Culvert Material	
<input type="radio"/> Box		<input type="checkbox"/> Metal	<input type="radio"/> Yes <input checked="" type="radio"/> No
<input type="radio"/> Pipe/Round		<input type="checkbox"/> Concrete	<input type="radio"/> Unknown
<input type="radio"/> Other:		<input type="checkbox"/> Plastic	Notes:
		<input type="checkbox"/> Stone/Masonry	
		<input type="checkbox"/> Other:	
Crossings Traversed (check all that apply)		Surrounding Habitat (check all that apply)	
<input type="checkbox"/> Bare ground	<input type="checkbox"/> Open vegetation	<input checked="" type="checkbox"/> Agricultural	<input type="checkbox"/> Grassland
<input type="checkbox"/> Rip-rap	<input type="checkbox"/> Closed vegetation	<input type="checkbox"/> Commercial	<input type="checkbox"/> Ranching
<input checked="" type="checkbox"/> Flowing water	<input type="checkbox"/> Railroad	<input type="checkbox"/> Residential-urban	<input type="checkbox"/> Riparian/wetland
<input type="checkbox"/> Standing water	<input type="checkbox"/> Road/trail - Type:	<input checked="" type="checkbox"/> Residential-rural	<input type="checkbox"/> Mixed use
<input type="checkbox"/> Seasonal water	<input type="checkbox"/> Other:	<input type="checkbox"/> Woodland/forested	<input type="checkbox"/> Other:
Areas Assessed (check all that apply)			
Check all areas that apply. If an area is not present in the structure, check the "not present" box.			
Document all bat indicators observed during the assessment. Include the species present, if known, and provide photo documentation as indicated.			
Area (check if assessed)	Assessment Notes	Evidence of Bats (include photos if present)	
<input checked="" type="checkbox"/> All crevices and cracks: Bridges/culverts: rough surfaces or imperfections in concrete Other structures: soffits, rafters, attic areas	<input type="checkbox"/> Not present	<input type="checkbox"/> Visual - live #	<input type="checkbox"/> dead #
		<input type="checkbox"/> Guano	<input type="checkbox"/> Audible
		<input type="checkbox"/> Staining	<input type="checkbox"/> Odor
			<input type="checkbox"/> Photos
			<input type="checkbox"/> Species
<input checked="" type="checkbox"/> Concrete surfaces (open roosting on concrete)	<input type="checkbox"/> Not present	<input type="checkbox"/> Visual - live #	<input type="checkbox"/> dead #
		<input type="checkbox"/> Guano	<input type="checkbox"/> Audible
		<input type="checkbox"/> Staining	<input type="checkbox"/> Odor
			<input type="checkbox"/> Photos
			<input type="checkbox"/> Species
<input checked="" type="checkbox"/> Spaces between concrete end walls and the bridge deck	<input type="checkbox"/> Not present	<input type="checkbox"/> Visual - live #	<input type="checkbox"/> dead #
		<input type="checkbox"/> Guano	<input type="checkbox"/> Audible
		<input type="checkbox"/> Staining	<input type="checkbox"/> Odor
			<input type="checkbox"/> Photos
			<input type="checkbox"/> Species
<input checked="" type="checkbox"/> Crack between concrete railings on top of the bridge deck	<input type="checkbox"/> Not present	<input type="checkbox"/> Visual - live #	<input type="checkbox"/> dead #
		<input type="checkbox"/> Guano	<input type="checkbox"/> Audible
		<input type="checkbox"/> Staining	<input type="checkbox"/> Odor
			<input type="checkbox"/> Photos
			<input type="checkbox"/> Species
<input checked="" type="checkbox"/> Vertical surfaces on concrete I-beams	<input type="checkbox"/> Not present	<input type="checkbox"/> Visual - live #	<input type="checkbox"/> dead #
		<input type="checkbox"/> Guano	<input type="checkbox"/> Audible
		<input type="checkbox"/> Staining	<input type="checkbox"/> Odor
			<input type="checkbox"/> Photos
			<input type="checkbox"/> Species
<input checked="" type="checkbox"/> Spaces between walls, ceiling joists	<input type="checkbox"/> Not present	<input type="checkbox"/> Visual - live #	<input type="checkbox"/> dead #
		<input type="checkbox"/> Guano	<input type="checkbox"/> Audible
		<input type="checkbox"/> Staining	<input type="checkbox"/> Odor
			<input type="checkbox"/> Photos
			<input type="checkbox"/> Species
<input checked="" type="checkbox"/> Weep holes, scupper drains, and inlets/pipes	<input type="checkbox"/> Not present	<input type="checkbox"/> Visual - live #	<input type="checkbox"/> dead #
		<input type="checkbox"/> Guano	<input type="checkbox"/> Audible
		<input type="checkbox"/> Staining	<input type="checkbox"/> Odor
			<input type="checkbox"/> Photos
			<input type="checkbox"/> Species
<input checked="" type="checkbox"/> All guiderails	<input type="checkbox"/> Not present	<input type="checkbox"/> Visual - live #	<input type="checkbox"/> dead #
		<input type="checkbox"/> Guano	<input type="checkbox"/> Audible
		<input type="checkbox"/> Staining	<input type="checkbox"/> Odor
			<input type="checkbox"/> Photos
			<input type="checkbox"/> Species
<input type="checkbox"/> All expansion joints	<input checked="" type="checkbox"/> Not present	<input type="checkbox"/> Visual - live #	<input type="checkbox"/> dead #
		<input type="checkbox"/> Guano	<input type="checkbox"/> Audible
		<input type="checkbox"/> Staining	<input type="checkbox"/> Odor
			<input type="checkbox"/> Photos
			<input type="checkbox"/> Species
Name: Carson Hoogewerf		Signature: <i>Carson Hoogewerf</i>	

Categorical Exclusion

Appendix D

**Section 106 of the National Historic
Preservation Act (NHPA)**

Minor Projects PA Project Submittal and Assessment Form

SECTION 1

Submittal of this form is only required for projects where Category B applies. Projects qualifying under Category A do not require submittal of this form. SECTION 2 (for Conditions of Category B.1 for curb/sidewalk) or SECTION 3 (for Conditions of Category B.9 for drainage structures) may be required as determined by INDOT-Cultural Resources Office (INDOT-CRO) review. INDOT-CRO will notify applicant if the Minor Projects PA does not apply.

Part I: Project Information-Completed by Applicant (Consultant/PM/Project Sponsor/INDOT District Staff)*

**A qualified professional historian (QP) is not required to complete Part I INDOT-Cultural Resources Office (INDOT-CRO) staff will be responsible for completion of Part II.*

Original Submission Date: June 2, 2023 **Amended Submission Date*:**

**Consult with INDOT-CRO to determine whether an amendment is required. For revisions/updates to original form, please detail in applicable sections below. Please use red font to distinguish the revisions/updates.*

Submitted By (Provide Name and Firm/Organization): Hannah Blad

Project Designation Number: 2003041

Route Number: N/A

Feature crossed (if applicable): Phillips Ditch

City/Township: Boone Township

County: Porter

Project Description:*

**Provide a full project description—include the same level of specificity and detail as expected in the NEPA document—in order to ensure a timely review by INDOT-CRO staff. For bridge and culvert projects, include specific details on the rehab or replacement including potential changes to width, height and materials. Be sure to include the specific elements listed below as applicable.*

The need for the project stems from the deteriorated condition of the existing bridge that carries County Road (CR) 250 West over Phillips Ditch. According to the structure inspection report dated May 12, 2021, the condition ratings of the bridge deck, superstructure, and substructure were 6 (“satisfactory”), 6 (“satisfactory”), and 5 (“fair”) out of 9, respectively. The bridge deck exhibited moderate seepage with efflorescence between beams. The superstructure exhibited efflorescence, beam deterioration, and widening. The substructure exhibited widening at the east abutment ends, deterioration at the north abutment including cracks and an offset footing, and spalling. The purpose of the project is to improve the overall condition of the structure and its components to a rating of at least 7 (“good”) out of 9.

The preferred alternative involves the removal of Bridge No. 64-00066 and construction of a new replacement bridge. The replacement structure is expected to be a single-span prestressed concrete box beam bridge featuring two 11-foot wide travel lanes with 4.0-foot shoulders and standard concrete railing. The clear roadway width and out to out width will be 30.0 feet and 33.0 feet, respectively, and will be approximately 63.75 feet in length. Approach work is expected to include full depth hot mix asphalt (HMA) for approximately 250 feet on either side of the bridge. The roadway section will include 11-foot lanes with 4.0-foot paved shoulders. Riprap drainage turnouts will be constructed in all four quadrants. New guardrail will be constructed along both approaches to the bridge. It is anticipated that 0.87 acre of new permanent right-of-way (ROW) and 0.07 acre of temporary (ROW) will be required for this project.

Minor Projects PA Project Submittal and Assessment Form

The maintenance of traffic (MOT) plan will include a full closure with detour route. The preliminary detour route will utilize CR W 1700 N, CR 125 W (S Baums Bridge Road), and CR W 900 S. The MOT will be implemented per the *Indiana Design Manual* guidelines. Construction is anticipated to begin in summer of 2026.

If the project includes any curb, curb ramp, or sidewalk work, please specify the location(s) of such work:

N/A

For bridge or small structure projects, please list feature crossed, structure number, NBI number, and structure type:

Feature Crossed: Phillips Ditch

Structure Number: 64-00066

NBI Number: 6400044

Structure Type: composite prestressed concrete box beam

For bridge projects, is the bridge included in INDOT's Historic Bridge Inventory (<https://www.in.gov/indot/2531.htm>)?

Yes No

If yes, did the inventory determine the bridge eligible for or listed in the National Register of Historic Places? Please provide page # of entry in Historic Bridge Inventory.

Yes No

Inventory Page # _____

Will there be right-of-way acquisition as part of this project?

Yes No

If yes was checked above, please check all that apply:

Permanent Temporary Reacquisition

If applicable, identify right-of-way acquisition locations in text below and in attached mapping. Please specify how much (both temporary and permanent) and indicate what activities are included in the proposed right-of-way:

Temporary ROW – 0.06 acres on east side of CR 250 West, including 0.05 acres north of the bridge and 0.01 acres south of the bridge, and 0.01 acres on west side of CR 250 West, south of the bridge

- Type of work in area
 - Driveway work and equipment access

Permanent ROW – 0.41 acres on east side of CR 250 West and 0.46 acres on the west side of CR 250 West

- Type of work
 - Construction access, grading, structure replacement, roadway widening, driveway work, riprap placement, and guardrail replacement

Minor Projects PA Project Submittal and Assessment Form

Is there any potential for additional temporary right-of-way to be needed later for purposes such as access, staging, etc.?

- Yes No

Archaeology (check one):

- All proposed activities are presumed to occur in previously disturbed soils***
**INDOT-CRO will notify you if project area includes undisturbed soils and requires an archaeological reconnaissance.*
- Project takes place in undisturbed soils and the archaeology report is included in submission or will be forthcoming***
** If an archaeology report is required, the Minor Projects PA Form will not be finalized until the report is reviewed and approved by INDOT-CRO. For INDOT-sponsored projects, INDOT-CRO may be able to complete the archaeological investigation. If you would like to request that INDOT-CRO complete an archaeological investigation, please contact the INDOT-CRO archaeology team lead. See CRM Pt. 1 Ch. 3 for current contact information.*

Please specify all applicable categories and condition(s) (highlight applicable conditions in yellow)*:

**Include full category text, including any conditions. INDOT-CRO will finalize categories upon their review.*

- A- 4. Roadway work associated with surface replacement, reconstruction, rehabilitation, or resurfacing projects, including overlays, shoulder treatments, pavement repair, seal coating, pavement grinding, and pavement marking within previously disturbed soils where replacement, repair, or installation of curbs, curb ramps or sidewalks will not be required.
- A-6. Repair, replacement, or upgrade of existing safety appurtenances such as guardrails, barriers, glare screens, and crash attenuators in previously disturbed soils.
- A-9. Installation, repair, or replacement of erosion control measures along roadways, waterways and bridge piers within previously disturbed soils.
- B-12. Replacement, widening, or raising the elevation of the superstructure on existing bridges, and bridge replacement projects (when both the superstructure and substructure are removed), under the following conditions [*BOTH Condition A, which pertains to Archaeological Resources, and Condition B, which pertains to Above-Ground Resources, must be satisfied*]:

Condition A (Archaeological Resources)

One of the two conditions listed below must be met (*EITHER Condition i or Condition ii must be satisfied*):

- i. Work occurs in previously disturbed soils; *OR*
- ii. Work occurs in undisturbed soils and an archaeological investigation conducted by the applicant and reviewed by INDOT Cultural Resources Office determines that no National Register-listed or potentially National Register-eligible archaeological resources are present within the project area. If the archaeological investigation locates National Register-listed or potentially National Register-eligible archaeological resources, then full Section 106 review will be required. Copies of any archaeological reports prepared for the project will be provided to the DHPA and any archaeological site form

Minor Projects PA Project Submittal and Assessment Form

information will be entered directly into the SHAARD by the applicant. The archaeological reports will also be available for viewing (by Tribes only) on INSCOPE.

Condition B (Above-Ground Resources)

The conditions listed below must be met (BOTH Condition i and Condition ii must be satisfied)

- i. Work does not occur adjacent to or within a National Register-listed or National Register-eligible district or individual above-ground resource; *AND*
- ii. With regard to the subject bridge, at least one of the conditions listed below is satisfied (*AT LEAST one of the conditions a, b or c, must be fulfilled*):
 - a. The latest Historic Bridge Inventory identified the bridge as non-historic (see <http://www.in.gov/indot/2531.htm>);
 - b. The bridge was built after 1945, and is a common type as defined in Section V. of the Program Comment Issued for Streamlining Section 106 Review for Actions Affecting Post-1945 Concrete and Steel Bridges issued by the Advisory Council on Historic Preservation on November 2, 2012 for so long as that Program Comment remains in effect *AND* the considerations listed in Section IV of the Program Comment do not apply;
 - c. The bridge is part of the Interstate system and was determined not eligible for the National Register under the Section 106 Exemption Regarding Effects to the Interstate Highway System adopted by the Advisory Council on Historic Preservation on March 10, 2005, for so long as that Exemption remains in effect.

Check if SECTION 2: Minor Projects PA Category B-1, Condition B-ii Submission is included

Check if SECTION 3: Minor Projects PA Category B-9, Condition B-i-c-2 or B-ii-b-3 Submission is included

Part II: Completed by INDOT-CRO

Amendments will be shown in red font.

Information reviewed (please check all that apply):

General project location map USGS map Aerial photograph Soil survey data

General project area photos Archaeology Reports Historic Property Reports

Indiana Historic Buildings, Bridges, and Cemeteries Map/Interim Report

Bridge inspection information/BIAS Historic Bridge Inventory Database

SHAARD SHAARD GIS Streetview Imagery County GIS Data/Property Cards

Other (please specify):

Curran, Michael J. and Andrew V. Martin

2023 A Phase Ia Archaeological Reconnaissance for the Proposed Replacement of Bridge 66 that carries South Country Road 250 West over Phillips Ditch, in Porter County, Indiana (INDOT Des. No. 2003041). Cultural Resources Analysts, Inc., Evansville. Document on file at INDOT-CRO.

Minor Projects PA Project Submittal and Assessment Form

IHSSI rating higher than contributing. None of the properties within the APE appear to rise to the level of significance to be considered National Register eligible either individually; nor does the potential for a historic district appear to exist.

With regard to the bridge itself, Porter County Bridge No. 00066 (NBI No. 6400044), it is a prestressed concrete box beam structure. While NBI information indicates the original construction date is 1930, a reconstruction in 1996 resulted in a superstructure replacement. Given the 1996 reconstruction, the bridge was excluded from evaluation in the Historic Bridge Inventory as not being old enough to make the cut-off of 1965. The presence of prestressed box beams confirm that a superstructure replacement occurred post-1965 because the first prestressed concrete bridge in Indiana was not built until 1952.

On November 2, 2012, the Advisory Council on Historic Preservation (ACHP) issued the *Program Comment for Streamlining Section 106 Review for Actions Affecting Post-1945 Concrete and Steel Bridges (Program Comment)*. The Program Comment relieves federal agencies from the Section 106 requirement to consider the effects of undertakings on most concrete and steel bridges built after 1945. On March 19, 2013, federal agencies were approved to use the Program Comment for Indiana projects.

The Program Comment applies for this bridge because it has not been previously listed in or determined eligible for listing in the National Register of Historic Places and it is not located in or adjacent to a historic district (Section IV.A of the Program Comment). As an example of a prestressed concrete box beam structure, this bridge is a common type and not one of the types to which the Program Comment does not apply (arch bridges, truss bridges, bridges with movable spans, suspension bridges, cable-stayed bridges, or covered bridges [Section IV.B]). Additionally, this bridge has not been identified as having exceptional significance for association with a person or event, being a very early or particularly important example of its type in the state or the nation, having distinctive engineering or architectural features that depart from standard designs, or displaying other elements that were engineered to respond to a unique environmental context (Section IV.C). This bridge also has not been identified as having some exceptional quality. Based on consultation between FHWA, INDOT, SHPO and interested parties, no bridges with exceptional significance were identified in Indiana (Section IV.C). Because the above criteria from the Program Comment have been met, no individual consideration under Section 106 is required for Porter County Bridge No. 00066.

Based on the available information, as summarized above, no above-ground concerns exist.

Archaeological Resources

An INDOT-CRO archaeologist who meets the Secretary of the Interior's Professional Qualification Standards as per 36 CFR Part 61 reviewed the Phase Ia field reconnaissance report completed for the project by Cultural Resources Analysts (Curran and Martin 2023). No archaeological sites were previously recorded within or adjacent to the project area.

A survey area measuring 2.4-hectares (6.1-acres), covering an area larger than the anticipated project limits, was examined through a combination of systematic visual surface survey, shovel probing ($n=3$), and visual inspection of disturbed areas. One archaeological site was documented within the project R/W as a result of the survey. Site 12PR866 is a lithic artifact scatter of indeterminate temporal and cultural affiliation. The author noted the site may extend beyond the survey and project area limits and, if so, this portion of the site remains unassessed. The portion of the site within the project R/W lacks the potential to provide important information and is not eligible for listing in the National Register of Historic Places (NRHP). No further investigation is recommended (Curran and Martin 2023).

The project has no potential to impact archaeological resources eligible for listing in the NRHP. Therefore, there are no archaeological concerns provided the project scope does not change.

Minor Projects PA Project Submittal and Assessment Form

Accidental Discovery: If any archaeological artifacts or human remains are uncovered during construction, demolition, or earth moving activities, construction within 100 feet of the discovery will be stopped, and INDOT-CRO and the Division of Natural Resources-Division of Historic Preservation and Archaeology (DNR-DHPA) will be notified immediately.

INDOT-CRO staff reviewer(s): Mary Kennedy and Dawn Alexander

INDOT Approval Date: 7/31/2023

Amendment Approval Date (if applicable):

****Be sure to attach this form to the National Environmental Policy Act documentation for this project. Also, the NEPA documentation shall reference and include the description of the specific stipulation in the PA that qualifies the project as exempt from further Section 106 review.*

**A PHASE IA ARCHAEOLOGICAL RECONNAISSANCE
FOR THE PROPOSED REPLACEMENT OF BRIDGE 66
THAT CARRIES SOUTH COUNTY ROAD 250 WEST OVER
PHILLIPS DITCH, IN PORTER COUNTY, INDIANA
(INDOT DES. NO. 2003041)**

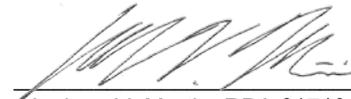
by
Michael J. Curran

Prepared for

Gary Quigg
Lochmueller Group, Inc.
6200 Vogel Road
Evansville, Indiana 47715
Phone: (317) 334-6803
Email: GQuigg@lochgroup.com

Prepared by

Cultural Resource Analysts, Inc.
201 NW 4th Street, Suite 204
Evansville, Indiana 47708
Phone: (812) 253-3009
Fax: (812) 253-3010
Email: amartin@crai-ky.com
CRA Project No.: I220269


Andrew V. Martin, RPA 61710
Principal Investigator

August 30, 2022

Lead Agency: Indiana Department of Transportation
INDOT Des. No.: 2003041
Ball State University, Applied Anthropology Laboratories Accession No.: 22.32

ABSTRACT

On July 27 and 28, 2022, Cultural Resource Analysts, Inc., personnel conducted a phase Ia archaeological survey for the proposed Bridge 66 replacement project in Porter County, Indiana (Indiana Department of Transportation Designation Number 2003041). In total, the survey area covers approximately 2.5 ha (6.1 acres) and was investigated by pedestrian survey, shovel testing, and visual inspection of obviously disturbed areas.

Prior to initiating the fieldwork, a records review was conducted utilizing data from the Indiana Division of Historic Preservation and Archaeology's State Historic Architectural and Archaeological Research Database. None of the previous archaeological investigations or archaeological sites are situated within the current survey area.

The current reconnaissance resulted in the identification of one previously unrecorded archaeological site (12PR866). Site 12PR866 is a prehistoric lithic artifact scatter of indeterminate temporal/cultural affiliation. The portion of Site 12PR866 within the survey area is recommended not eligible for inclusion in the National Register of Historic Places. No further archaeological work is recommended for the project.

Categorical Exclusion

Appendix E

**Red Flag Investigation
& Hazardous Materials**



Date: October 28, 2022

To: Site Assessment & Management (SAM)
Environmental Policy Office - Environmental Services Division (ESD)
Indiana Department of Transportation (INDOT)
100 N Senate Avenue, Room N758-ES
Indianapolis, IN 46204

From: Ruth Hook, CPESC, CESSWI
Lochmueller Group
112 W Jefferson Blvd
South Bend, IN
rhook@lochgroup.com

Re: RED FLAG INVESTIGATION
DES # 2003041, Local Project
Bridge Project
CR 250 W over Phillips Ditch, 0.50 Mile South of CR 900 S
Porter County, Indiana

PROJECT DESCRIPTION

Brief Description of Project: The project involves the replacement of Porter County Bridge #66 (Bridge No. 64-00066) which carries CR 250 W over Phillips Ditch, 0.50 mile south of CR 900S. Phillips Ditch is also referred to as Cornell Ditch on the IDEM 303d mapper; however, the use of Phillips Ditch will be used throughout this document as well as throughout the environmental document process. The project is located in Boone Township, Porter County. The existing structure is a single span pre-stressed concrete adjacent box-beam structure with an overall length of 52.5 feet. Project details and design are still under development. The proposed new structure is anticipated to be a three-span reinforced concrete slab. Work along CR 250 W will be required to raise the roadway profile to the new vertical profile of the bridge. Guardrail along CR 250 W will be removed and replaced to meet current standards. The field entrance located in the northwest quadrant of the project area will likely be relocated further north along CR 250 W to accommodate guardrail improvements. Riprap placement within Phillips Ditch is anticipated and will be finalized following the completion of hydraulic analysis.

Excavation within Philips Ditch for riprap placement and pier construction is anticipated to be between 3 and 4 feet below ground surface. Excavation along CR 250 W for roadway reconstruction, guardrail placement, and other roadway improvements is anticipated to be no greater than 4 feet below ground surface.

Bridge Work Included in Project: Yes No Structure #(s) 64-00066

If this is a bridge project, is the bridge Historical? Yes No , Select Non-Select

(Note: If the project involves a historical bridge, please include the bridge information in the Recommendations Section of the report).

Culvert Work Included in Project: Yes No Structure #(s) _____

Proposed right of way: Temporary # Acres _____, Permanent # Acres >0.5 acre, Not Applicable

Type and proposed depth of excavation: Excavation within Phillips Ditch for riprap placement and pier construction is anticipated to be between 3 and 4 feet below ground surface. Excavation along CR 250 W for roadway reconstruction, guardrail placement, and other roadway improvements is anticipated to be no greater than 4 feet below ground surface. Maintenance of traffic (MOT): Project details are still under development. The MOT is anticipated to utilize a closure with a signed detour. The detour has yet to be finalized but is likely to utilize CR 1700 N, CR 625 W, CR 1000 S, CR 500 W, and SR 900 S.

Work in waterway: Yes No Below ordinary high water mark: Yes No

State Project: LPA:

Any other factors influencing recommendations: N/A

INFRASTRUCTURE TABLE AND SUMMARY

Infrastructure			
Indicate the number of items of concern found within the 0.5 mile search radius. If there are no items, please indicate N/A:			
Religious Facilities	N/A	Recreational Facilities	N/A
Airports ¹	N/A	Pipelines	N/A
Cemeteries	N/A	Railroads	N/A
Hospitals	N/A	Trails	N/A
Schools	N/A	Managed Lands	N/A

¹In order to complete the required airport review, a review of public-use airports within 3.8 miles (20,000 feet) is required.

Explanation: No infrastructure resources were identified within the 0.5 mile search radius.

WATER RESOURCES TABLE AND SUMMARY

Water Resources			
Indicate the number of items of concern found within the 0.5 mile search radius. If there are no items, please indicate N/A:			
NWI - Points	N/A	Canal Routes - Historic	N/A
Karst Springs	N/A	NWI - Wetlands	15
Canal Structures – Historic	N/A	Lakes	2
NPS NRI Listed	N/A	Floodplain - DFIRM	2
NWI-Lines	2	Cave Entrance Density	N/A
IDEM 303d Listed Streams and Lakes (Impaired)	7	Sinkhole Areas	N/A
Rivers and Streams	13	Sinking-Stream Basins	N/A

Explanation:

NWI – Line: Two (2) NWI – Line segments are located within the 0.5 mile search radius. The nearest NWI – Line segment is located approximately 0.35 mile east of the project area. No impact is expected.

IDEM 303d Listed Streams (Impaired): Seven (7) 303d listed stream segments are located within the 0.5 mile search radius. Phillips Ditch is located within the project area. Phillips Ditch is listed as impaired for *E. coli*. Workers who are working in or near water with *E. coli* should take care to wear appropriate PPE, observe proper hygiene procedures, including regular hand washing, and limit personal exposure.

Rivers and Streams: Thirteen (13) river and stream segments are located within the 0.5 mile search radius. Phillips Ditch is located within the project area. A Waters of the US report is recommended based on mapped features, and coordination with the appropriate agency, if applicable, will occur.

NWI - Wetlands: Fifteen (15) NWI – Wetlands are located within the 0.5 mile search radius. One (1) wetland is located approximately 0.02 mile north of the project area. A Waters of the US report is recommended based on mapped features, and coordination with the appropriate agency, if applicable, will occur.

Lakes: Two (2) lakes are located within the 0.5 mile search radius. One (1) lake is located approximately 0.02 mile north of the project area. A Waters of the US report is recommended based on mapped features, and coordination with the appropriate agency, if applicable, will occur.

Floodplains: Two (2) floodplain polygons are located within the 0.5 mile search radius. The project area is located within one (1) of the floodplain polygons. Coordination with the appropriate agency will occur.

MINING AND MINERAL EXPLORATION TABLE AND SUMMARY

Mining/Mineral Exploration			
Indicate the number of items of concern found within the 0.5 mile search radius. If there are no items, please indicate N/A:			
Petroleum Wells	N/A	Mineral Resources	N/A
Mines – Surface	N/A	Mines – Underground	N/A

Explanation: No mining/mineral exploration resources were identified within the 0.5 mile search radius.

HAZARDOUS MATERIAL CONCERNS TABLE AND SUMMARY

Hazardous Material Concerns			
Indicate the number of items of concern found within the 0.5 mile search radius. If there are no items, please indicate N/A:			
Superfund	N/A	Manufactured Gas Plant Sites	N/A
RCRA Generator/ TSD	N/A	Open Dump Waste Sites	N/A
RCRA Corrective Action Sites	N/A	Restricted Waste Sites	N/A
State Cleanup Sites	N/A	Waste Transfer Stations	N/A
Septage Waste Sites	N/A	Tire Waste Sites	N/A
Underground Storage Tank (UST) Sites	N/A	Confined Feeding Operations (CFO)	N/A
Voluntary Remediation Program	N/A	Brownfields	N/A
Construction Demolition Waste	N/A	Institutional Controls	N/A

Solid Waste Landfill	N/A	NPDES Facilities	N/A
Infectious/Medical Waste Sites	N/A	NPDES Pipe Locations	N/A
Leaking Underground Storage (LUST) Sites	N/A	Notice of Contamination Sites	N/A

Unless otherwise noted, site specific details presented in this section were obtained from documents reviewed on the Indiana Department of Environmental Management (IDEM) Virtual File Cabinet (VFC).

Explanation: No hazardous material concerns were identified with the 0.5 mile search radius.

ECOLOGICAL INFORMATION SUMMARY

The Porter County listing of the Indiana Natural Heritage Data Center information on endangered, threatened, or rare (ETR) species and high quality natural communities is provided at (https://www.in.gov/dnr/nature-preserves/files/np_porter.pdf). A preliminary review of the Indiana Natural Heritage Database by INDOT ESD did not indicate the presence of ETR species within the 0.5 mile search radius. Coordination with USFWS and IDNR will occur.

A review of the USFWS database did not indicate the presence of endangered bat species in or within 0.5 mile of the project area. The project area is located in a rural area surrounded by agriculture and residential properties. The May 12, 2021 inspection report for Bridge #64-00066 contains no information about whether bats are present or absent on the bridge. Additional investigation to confirm the presence or absence of bats on the bridge will be necessary. The range-wide programmatic consultation for the Indiana Bat and Northern Long-eared Bat will be completed according to the most recent "Using the USFWS's IPaC System for Listed Bat Consultation for INDOT Projects".

RECOMMENDATIONS SECTION

Include recommendations from each section. If there are no recommendations, please indicate N/A:

INFRASTRUCTURE: N/A

WATER RESOURCES: A Waters of the US Report is recommended based on mapped features and coordination with the appropriate agency, if applicable, will occur for the following features:

- One (1) stream, Phillips Ditch, flows through the project area
- One (1) wetland is located approximately 0.02 mile north of the project area
- One (1) lake is located approximately 0.02 mile north of the project area
- The project area is located within a floodplain (coordination only)

Phillips Ditch is located within the project area. Phillips Ditch is listed as impaired for *E. coli*. Workers who are working in or near water with *E. coli* should take care to wear appropriate PPE, observe proper hygiene procedures, including regular hand washing and limit personal exposure.

MINING/MINERAL EXPLORATION: N/A

HAZARDOUS MATERIAL CONCERNS: N/A

ECOLOGICAL INFORMATION: Coordination with USFWS and IDNR will occur. Additional investigation to determine the presence or absence of bats under the bridge will be necessary. The range-wide programmatic consultation for the

Indiana Bat and Northern Long-eared Bat will be completed according to the most recent "Using the USFWS's IPaC System for Listed Bat Consultation INDOT Projects".

INDOT ESD concurrence: Darlane Davis Digitally signed by Darlane Davis
Date: 2022.10.31 15:37:08 -04'00' (Signature)

Prepared by:



Ruth Hook, CPESC, CESSWI
Lochmueller Group

Graphics:

A map for each report section with a 0.5 mile search radius buffer around all project area(s) showing all items identified as possible items of concern is attached. If there is not a section map included, please change the YES to N/A:

SITE LOCATION: YES

Removed to avoid duplication. See Appendix B.

INFRASTRUCTURE: N/A

WATER RESOURCES: YES

MINING/MINERAL EXPLORATION: N/A

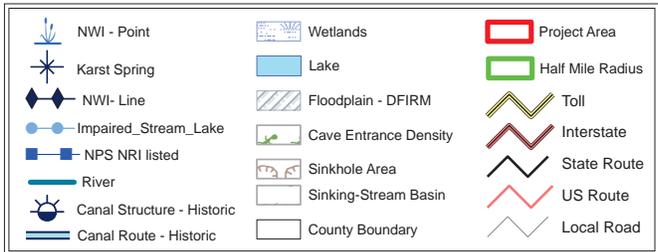
HAZARDOUS MATERIAL CONCERNS: N/A

Red Flag Investigation - Water Resources
 CR 250 W over Phillips Ditch, 0.50 Mile South of CR 900 S
 Des. No. 2003041, Bridge Replacement Project
 Porter County, Indiana



Sources:
Non Orthophotography
 Data - Obtained from the State of Indiana Geographical Information Office Library
Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)
 Map Projection: UTM Zone 16 N Map Datum: NAD83

This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.



Categorical Exclusion
Appendix F
Water Resources

**Waters of the U.S. Determination
 County Road 250 West over Phillips Ditch
 Bridge Replacement Project
 Porter County, Indiana
 Des. No. 2003041**

Date of Field Reconnaissance:
 August 2, 2022

Project Location

The project is located along County Road (CR) 250 West, 0.5 mile south of CR 900 South (Pages A1 to A3).

- Section 21, Township 33 North, Range 6 West
- Kouts 1:24,000 United States Geological Survey (USGS) Quadrangles
- Boone Township, Porter County, Indiana
- Latitude: 41.29551, Longitude: -87.112679

Project Description

The proposed project (Des. No. 2003041) will involve replacement of the existing structure, (Bridge No. 64-00066, NBI No. 6400066). The existing structure is a single span pre-stressed concrete adjacent box-beam bridge with a total length of 52.5 feet. The replacement structure is expected to be a three-span concrete slab bridge or a single span pre-stressed concrete beam bridge.

Two streams, Phillips Ditch and UNT 1 to Keller Ditch, were identified within the investigation area. Surrounding landscape consists of maintained and unmaintained roadside vegetation, row crop agriculture, and residential properties. The investigation area is partially within the Indiana Department of Natural Resources (IDNR) regulated floodway of Phillips Ditch.

Soils

According to the 2020 USDA Soil Survey Geographic (SSURGO) Database for Porter County, the investigation area contains soil areas considered to be hydric (Page A4).

Soil Summary Table

Soil Unit Name	Symbol	NRCS Flood Frequency	NRCS Drainage Class	NRCS Hydric Soil Category	SSURGO Hydric Rating
Bourbon Sandy Loam	Br	None	Somewhat poorly drained	Predominantly nonhydric (1-32%)	10% hydric
Hanna Sandy Loam, 0 to 3% slopes	HaA	None	Moderately well drained	Predominantly nonhydric (1-32%)	10% hydric
Brems Sand, 0 to 3% slopes	BtA	None	Moderately well drained	Predominantly nonhydric (1-32%)	10% hydric
Tyner Loamy Sand, 0 to 3% slopes	TyA	None	Excessively drained	Predominantly nonhydric (1-32%)	10% hydric
Pinhook Loam	Ph	None	Poorly drained	Hydric (100%)	100% hydric
Plainfield sand, 2 to 6% slopes	PIB	None	Excessively drained	Predominantly nonhydric (1-32%)	1% hydric



Soil Unit Name	Symbol	NRCS Flood Frequency	NRCS Drainage Class	NRCS Hydric Soil Category	SSURGO Hydric Rating
Sebewa loam, shaly substratum	Sb	None	Very poorly drained	Hydric (100%)	100% hydric

National Wetland Inventory Information

Two National Wetland Inventory (NWI) wetland was identified within the investigation area (Page A5). The U.S. Fish and Wildlife Indiana wetlands geodatabase identified two riverine, lower perennial, unconsolidated bottom, semi permanently flooded, excavated (R2UBFx) wetlands associated with Phillips Ditch and UNT 1 to Keller Ditch. Wetland types are based on *Classification of Wetlands and Deepwater Habitats of the United States (Cowardin et al. 1979)*.

NWI Summary Table

Wetland Type	Description	Location: Lat/Long
R2UBFx (Phillips Ditch)	Riverine, lower perennial, unconsolidated bottom, semi permanently flooded, excavated	41.295466° -87.112682°
R2UBFx (UNT1 to Keller Ditch)	Riverine, lower perennial, unconsolidated bottom, semi permanently flooded, excavated	41.296406° -87.112932°

12-Digit HUC and Flood Zones

The Porter Co. Bridge 66 Project investigation area is within the 071200011009 (Cornell Ditch-Phillips Ditch) and 071200011008 (Ahlgrim Ditch) 12-digit HUCs (Pages A2 and A3). The USGS StreamStats website (<https://streamstats.usgs.gov/ss/>) generated two watersheds within the investigation area. Phillips Ditch watershed has an upstream drainage area of 19.053 square miles. UNT 1 to Keller Ditch watershed has an upstream drainage area of 0.285 square mile. The Indiana Floodplain Information Portal ([IDNR Floodplain Portal](#)) Best Available Flood Zones data indicate that the investigation area is within the regulated floodway of Phillips Ditch (Page A7).

Attached Documents

- Location Map
- USGS Quad Map (1:24,000)
- USGS Quad Map (1:12,000)
- USDA SSURGO Hydric Soils Map
- USFWS NWI Map
- StreamStats Watershed Map
- IDNR Best Available Flood Hazard & NHD Line Map
- Water Resources Map
- Photo Location Map and Project Photos
- Preliminary Jurisdictional Determination Form

Field Reconnaissance

The Waters of the U.S. (WOTUS) investigation area limits were established based on the scope of work expected for the Porter Co. Bridge 66 Project. Two streams were identified during the field reconnaissance. Although soils within the investigation area were mapped as hydric, no clear signs of



wetlands were present. Weather on the day of the field investigation was sunny, and the last precipitation event in the area occurred on July 28 at 0.37 inches according to NOAA Climate Data (<https://www.weather.gov/wrh/Climate?wfo=lot>).

Water resource boundaries were mapped using a Trimble R1 receiver (sub-meter accuracy) and ArcCollector as the GIS data collection platform. For those features that displayed bed and bank, the ordinary high water mark (OHWM) width and depth were measured at the maximum dimension observed beyond the influence of bridge and culvert structures. OHWM measurements were also documented for any stream features observed in the field that were not included as blue-line or National Hydrography Dataset (NHD) features.

Stream Features

According to the USGS Kouts, 1:24,000 topographic quadrangle, one solid blue-line and one dashed blue line features are present within the investigation area (Pages A2 and A3). Those features were field identified as Phillips Ditch and UNT 1 to Keller Ditch, respectively. The NHD GIS dataset included two classified NHD flowlines (Page A7). The field investigation concluded that these flowlines represent Phillips Ditch and UNT 1 to Keller Ditch. Three unclassified NHD flowlines were also present within the investigation area, none of which represented identifiable features in the field.

Phillips Ditch

Phillips Ditch is a perennial stream that flows from northeast to southwest through the investigation area. This reach of Phillips Ditch has been excavated and channelized to convey agricultural drainage. The stream has a moderate flow. The banks are steep but well vegetated and have minimal signs of scour and undercutting. Approximately 336 feet of the stream is within the investigation area. The OHWM is 22.5 feet wide by 0.67 feet deep. The upstream drainage area is 19.053 square miles. The substrate within this reach of Phillips Ditch is silt (60%) and sand (40%). The channel morphology within this reach of Phillips Ditch is predominately run (90%) with some pools present (10%).

Phillips Ditch is considered a perennial, relatively permanent water (RPW) with a downstream connection to Kankakee River, which is navigable throughout Porter County, located approximately 0.87 river mile southwest of the investigation area. Phillips Ditch would be subject to U.S. Army Corps of Engineers (USACE) jurisdiction under Section 404 of the Clean Water Act due to its connection to a Traditional Navigable Water (TNW) downstream of the project. This stream is not subject to USACE jurisdiction under Section 10 of the River and Harbors Act.

UNT 1 to Keller Ditch

UNT 1 to Keller Ditch is a constructed intermittent stream that flows from north to south and then turns west to head outside the investigation area. This reach of UNT 1 to Keller Ditch has been excavated and channelized to convey agricultural drainage. It has steep, heavily vegetated banks. It is an intermittent stream that is vegetated in portions with a defined bed and bank. Instream vegetation included common duckweed (*Lemna minor*) and narrow leaf cattail (*Typha angustifolia*). Approximately 514 feet of the

stream is within the investigation area. The OHWM is 10 feet wide by 0.5 feet deep. The upstream drainage area is 0.285 square mile. The substrate within this reach of UNT 1 to Keller Ditch is muck (100%).

UNT 1 to Keller Ditch is considered an intermittent, RPW with a downstream connection to the Kankakee River via Keller Ditch. UNT 1 to Keller Ditch would be subject to USACE jurisdiction under Section 404 of the Clean Water Act due to its connection to a TNW downstream of the project. This stream is not subject to USACE jurisdiction under Section 10 of the River and Harbors Act.

Stream Summary Table

Feature Name	Photos	Lat/Long	Length (ft)	OHW Width (ft)	OHW Depth (ft)	USGS Blue Line? Type?	Riffles? Pools?	Quality	Substrate	Likely Waters of the U.S.?
Phillips Ditch	12, 16-20	41.29550°N -87.11270°W	336	22.5	0.67	Yes Solid	No Yes	Average	Sand Silt	Yes
UNT 1 to Keller Ditch	27, 28, 31-35, 37	41.29700°N -87.11280°W	514	10.0	0.5	Yes Dashed	No No	Low	Muck	Yes

Roadside Ditch Features

Roadside ditch features were not identified within the investigation area.

Wetland Features

Wetland features were not identified within the investigation area.

Open Water Features

Open water features were not identified within the investigation area.

Conclusion

The August 2, 2022, field review for the CR 250 W over Phillips Ditch Bridge Project identified two stream features, Philip Ditch and UNT 1 to Keller Ditch, within the investigation area. Phillips Ditch, and UNT 1 to Keller Ditch would be subject to USACE jurisdiction under Section 404 of the Clean Water Act due to their connection to the Kankakee River downstream of the project area.

Every effort should be taken to avoid and minimize impacts to stream features. If impacts are necessary, then mitigation may be required. The final determination of jurisdictional waters is ultimately made by the USACE. This report is our best judgment based on the guidelines set forth by the Corps.

All drainage structures within the investigation area were examined on August 2, 2022, for the presence of bats and migratory birds and were found to show no direct or indirect signs of occupation.

Acknowledgement

This waters determination has been prepared based on the best available information, interpreted in the light of the investigator’s training, experience, and professional judgement in conformance with the 1987



Corps of Engineers Wetlands Delineation Manual, the appropriate regional supplement, the USACE Jurisdictional Determination Form Instructional Guidebook, and other appropriate agency guidelines.

Ruth Hook, CPESC, CESSWI



Environmental Team Lead – Northern Indiana
Lochmueller Group, Inc.

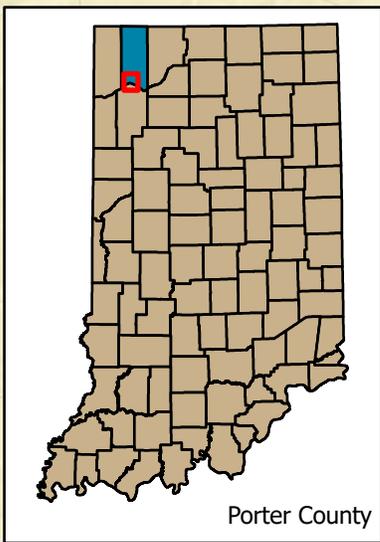
Preparers

Lochmueller Group, Inc. Staff	Position	Contributing Effort
Carson Hoogewerf	Environmental Specialist	Field Data Collection
Ruth Hook	Environmental Lead	Field Data Collection & Report Preparation

Report Completed Date:
September 19, 2022



Ludington Ditch



Survey Area

Legend
 Survey Area

Source(s): Esri, HERE, Garmin, GeoTechnologies, Inc., NGA, USGS,

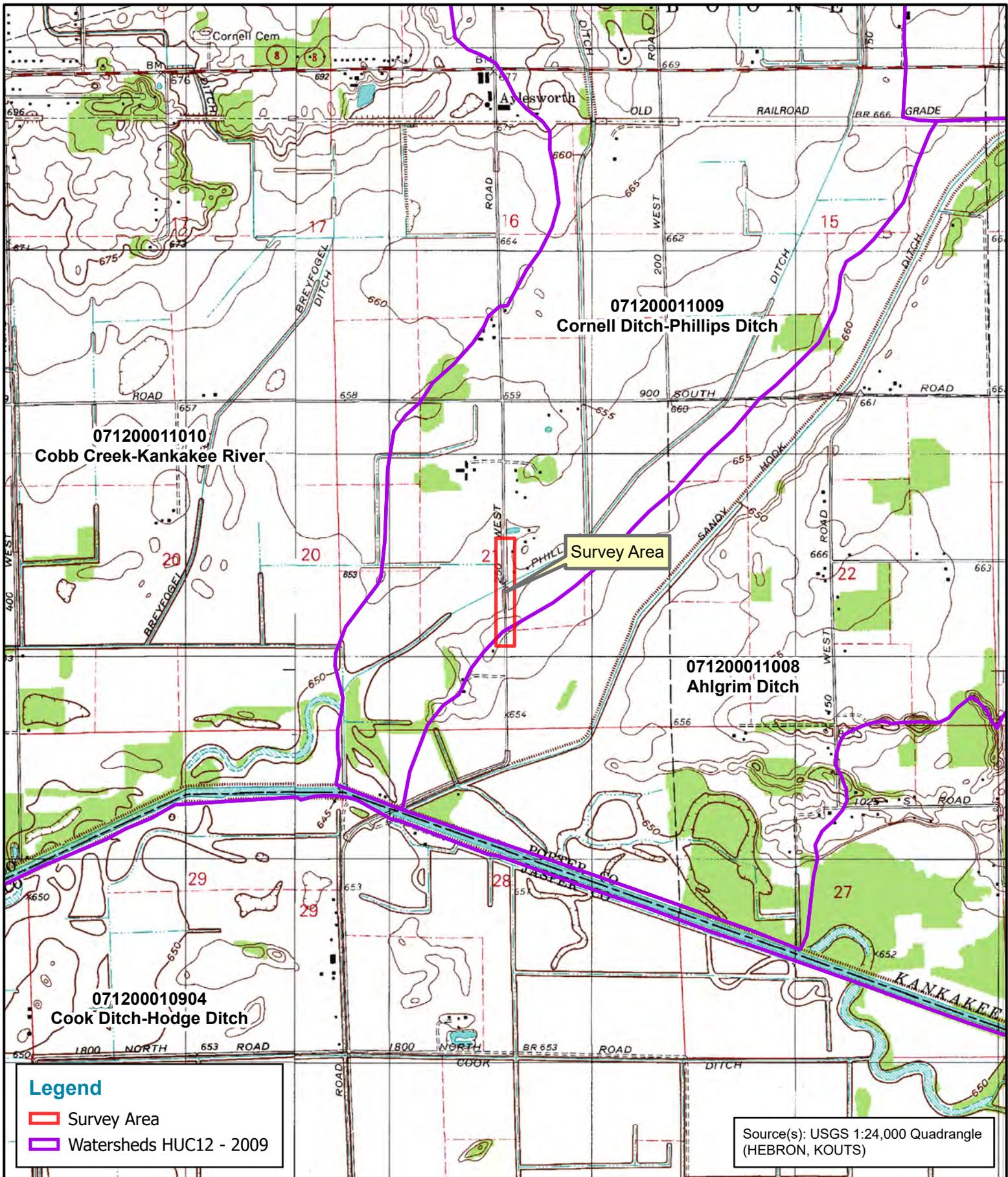
LOCHMUELLER GROUP
 3502 Woodview Trace, Suite 150
 Indianapolis, IN 46268
 Phone: (317) 222-3880
 Fax: (317) 222-3881

Location Map
 Des. No. 2003041
 Waters of the U.S. Report

0 0.5 1 Miles

County: Porter
 Township: Boone
 State: Indiana

Porter Co. Bridge 66
 Bridge Replacement
 Created: 8/31/2022, RHook



Legend

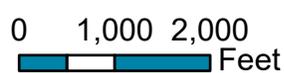
- Survey Area
- Watersheds HUC12 - 2009

Source(s): USGS 1:24,000 Quadrangle (HEBRON, KOUTS)



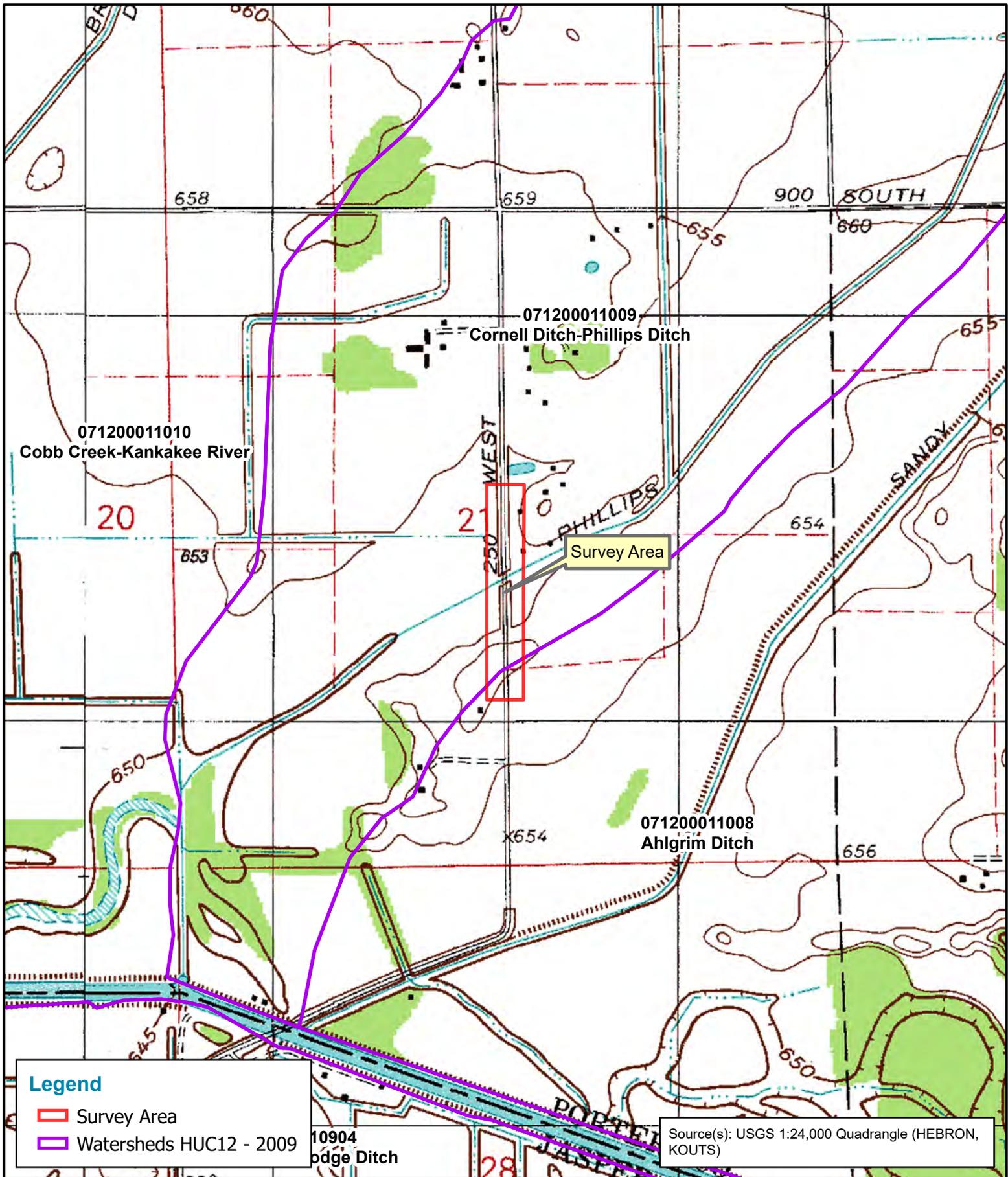
3502 Woodview Trace, Suite 150
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USGS Quad Map (1:24,000)
 Des. No. 2003041
 Waters of the U.S. Report



County: Porter
 Township: Boone
 State: Indiana

Porter Co. Bridge 66
 Bridge Replacement
 Created: 8/31/2022, RHook



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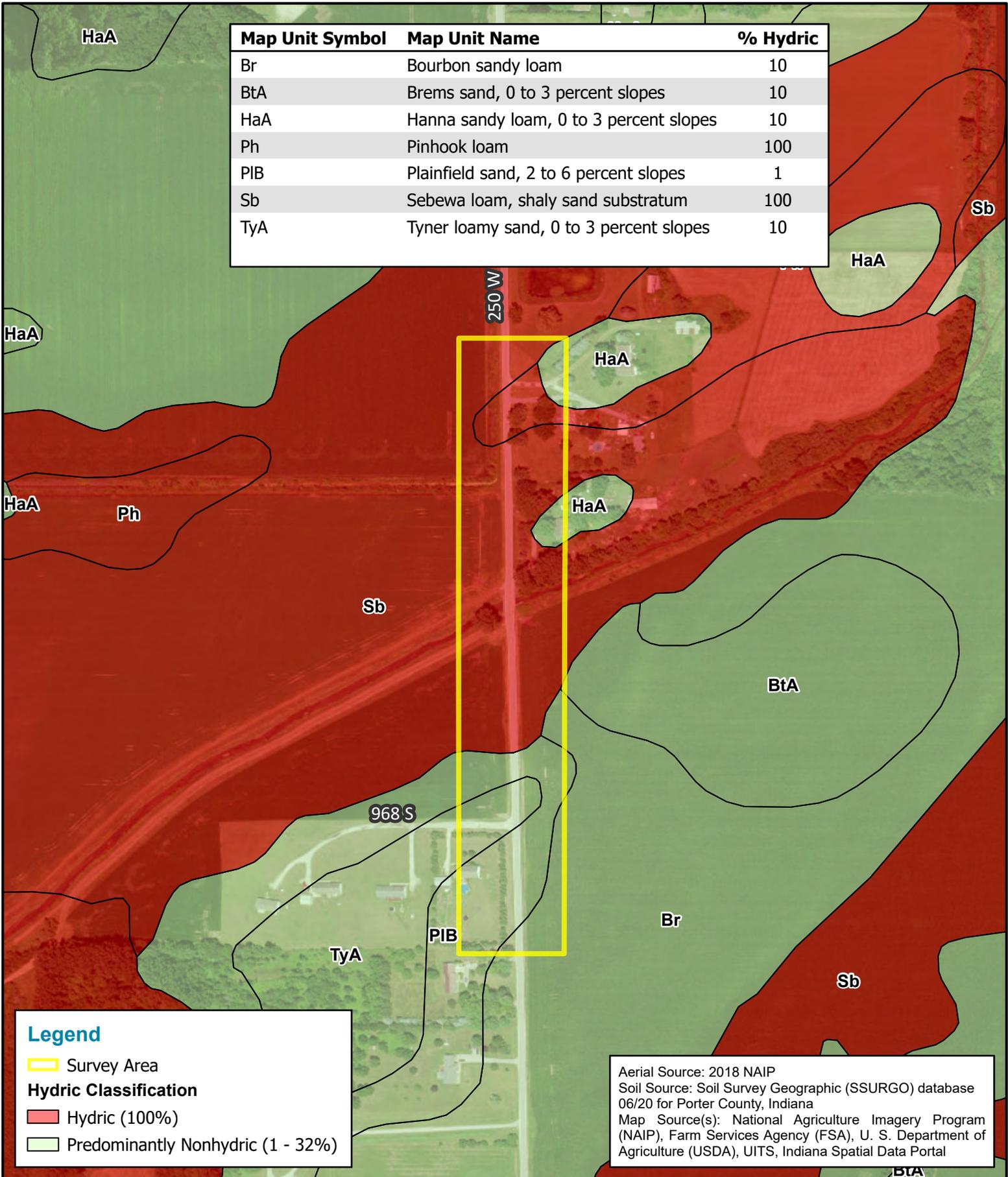
USGS Quad Map (1:12,000)
Des. No. 2003041
Waters of the U.S. Report

0 500 1,000 Feet

County: Porter
Township: Boone
State: Indiana

Porter Co. Bridge 66
Bridge Replacement
Created: 8/31/2022, RHook

Map Unit Symbol	Map Unit Name	% Hydric
Br	Bourbon sandy loam	10
BtA	Brems sand, 0 to 3 percent slopes	10
HaA	Hanna sandy loam, 0 to 3 percent slopes	10
Ph	Pinhook loam	100
PIB	Plainfield sand, 2 to 6 percent slopes	1
Sb	Sebewa loam, shaly sand substratum	100
TyA	Tyner loamy sand, 0 to 3 percent slopes	10



Legend

Survey Area

Hydric Classification

Hydric (100%)

Predominantly Nonhydric (1 - 32%)

Aerial Source: 2018 NAIP
 Soil Source: Soil Survey Geographic (SSURGO) database 06/20 for Porter County, Indiana
 Map Source(s): National Agriculture Imagery Program (NAIP), Farm Services Agency (FSA), U. S. Department of Agriculture (USDA), UITS, Indiana Spatial Data Portal

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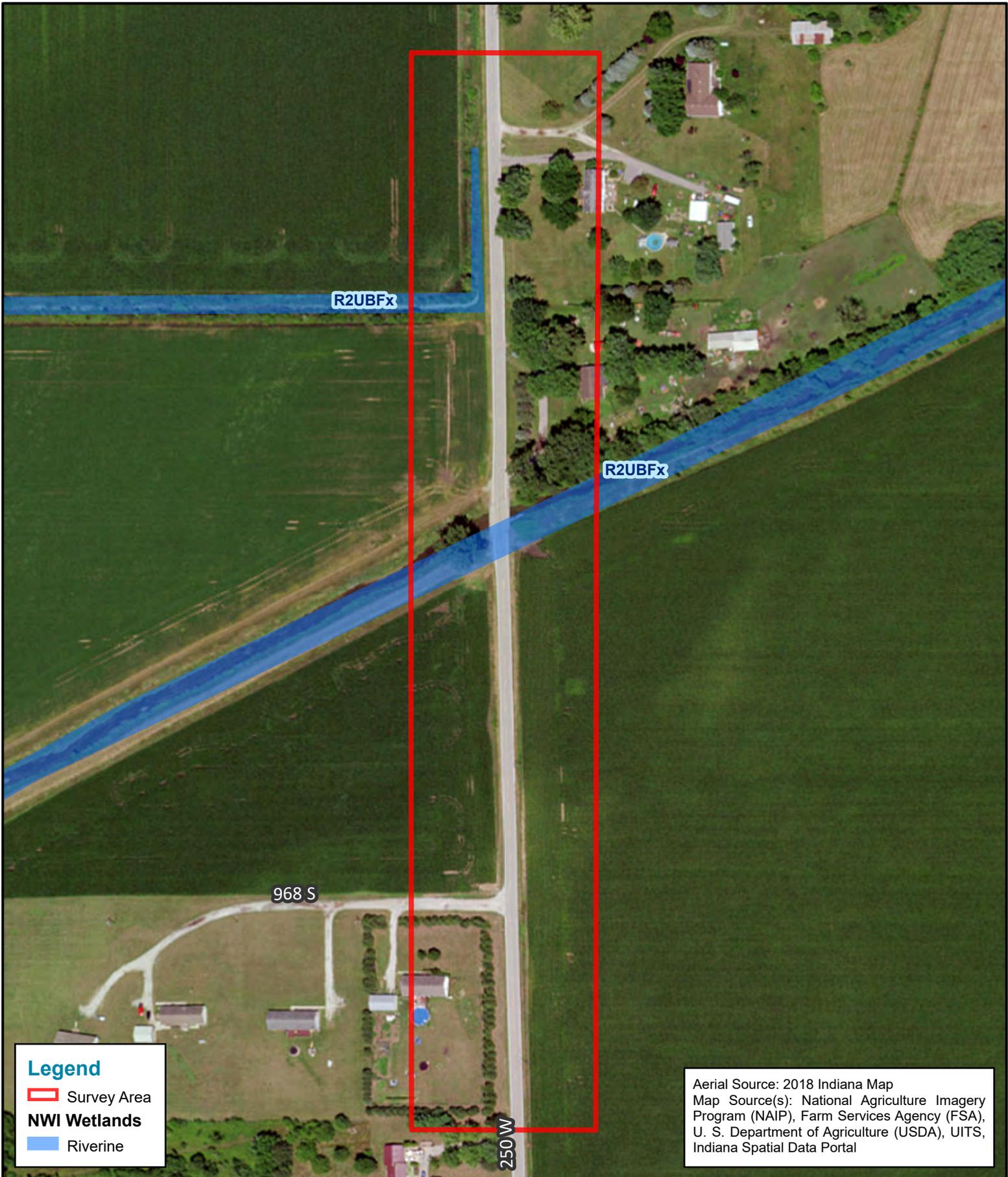
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USDA SSURGO Soils Map
 Des. No. 2003041
 Waters of the U.S. Report

0 175 350 Feet

County: Porter
 Township: Boone
 State: Indiana

Porter Co. Bridge 66
 Bridge Replacement
 Created: 8/31/2022, RHook



Legend

- Survey Area
- NWI Wetlands**
- Riverine

Aerial Source: 2018 Indiana Map
 Map Source(s): National Agriculture Imagery Program (NAIP), Farm Services Agency (FSA), U. S. Department of Agriculture (USDA), UITS, Indiana Spatial Data Portal

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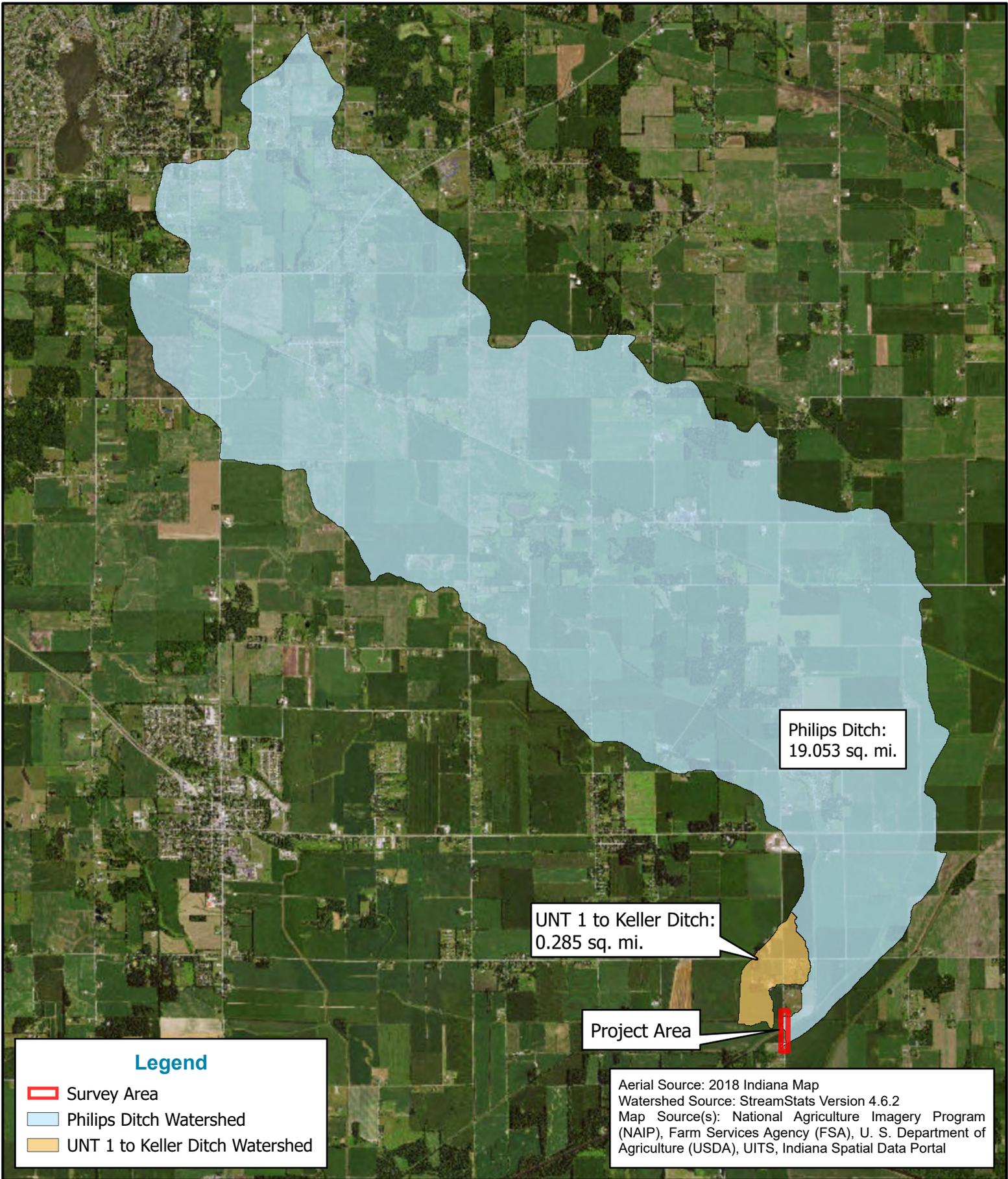
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USFWS NWI Map
 Des. No. 2003041
 Waters of the U.S. Report

0 100 200
 Feet

County: Porter
 Township: Boone
 State: Indiana

Porter Co. Bridge 66
 Bridge Replacement
 Created: 8/31/2022, RHook



Legend

-  Survey Area
-  Philips Ditch Watershed
-  UNT 1 to Keller Ditch Watershed

UNT 1 to Keller Ditch:
0.285 sq. mi.

Philips Ditch:
19.053 sq. mi.

Project Area

Aerial Source: 2018 Indiana Map
 Watershed Source: StreamStats Version 4.6.2
 Map Source(s): National Agriculture Imagery Program (NAIP), Farm Services Agency (FSA), U. S. Department of Agriculture (USDA), UITS, Indiana Spatial Data Portal



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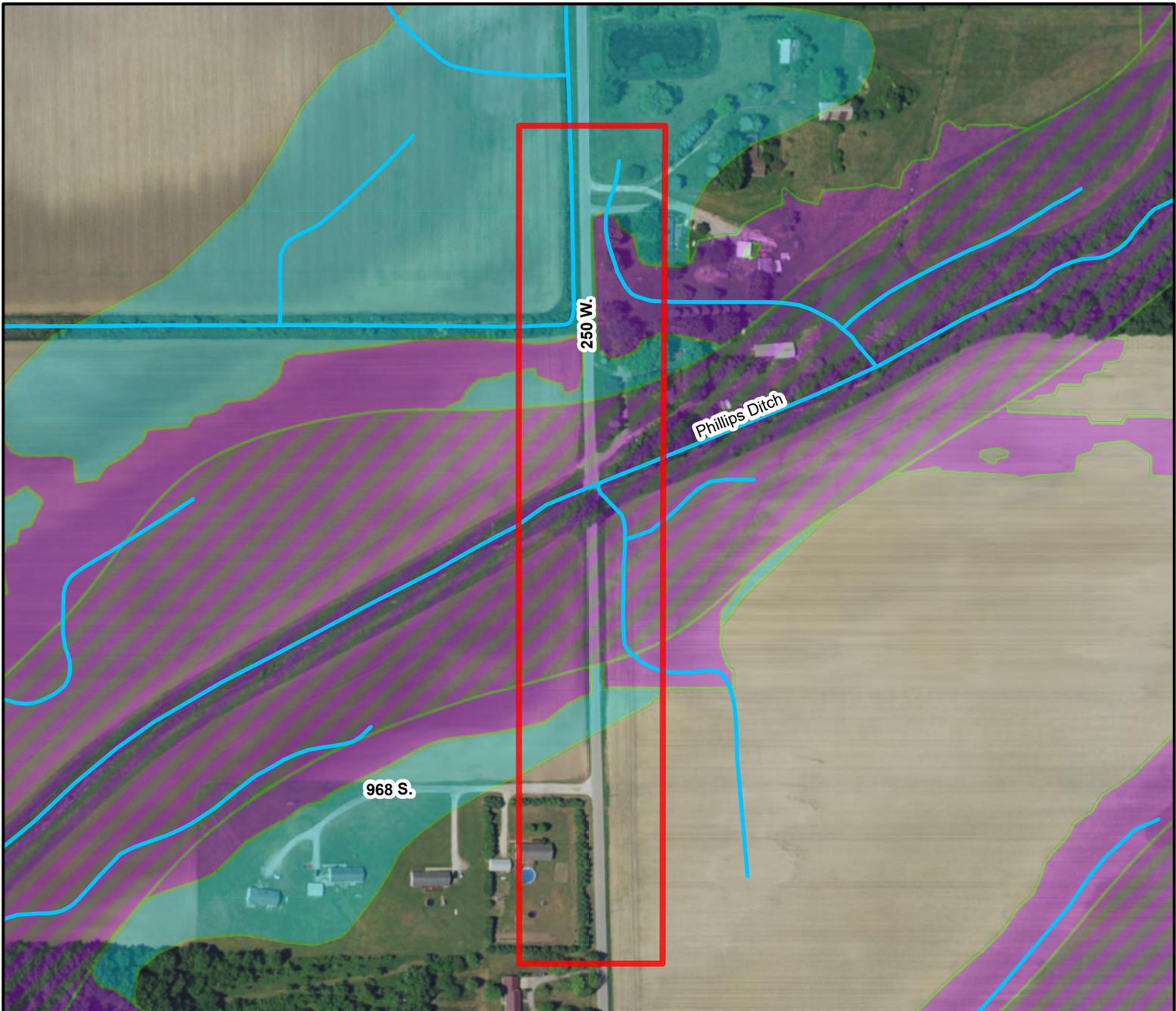
StreamStats Watershed Map
 Des. No. 2003041
 Waters of the U.S. Report

0 0.5 1 Miles



County: Porter
 Township: Boone
 State: Indiana

Porter Co. Bridge 66
 Bridge Replacement
 Created: 8/31/2022, RHook



Legend

Survey Area	DNR Detailed Fringe
NHD Flowlines	DNR Approximate Fringe
<all other values>	Additional Floodplain Area; DNR .2 Percent Flood Hazard
FEMA Zone AE Floodway; FEMA Administrative Floodway	FEMA Protected by Levee
DNR Detailed Floodway	FEMA Floodplain - Ponding (Depth)
DNR Approximate Floodway	FEMA Floodplain - Sheet Flow (Depth)
FEMA Zone A	Not Mapped
FEMA Zone AE	

Aerial Source: 2018 Indiana Map
 Map Source(s): Indiana Department of Natural Resources, Division of Water

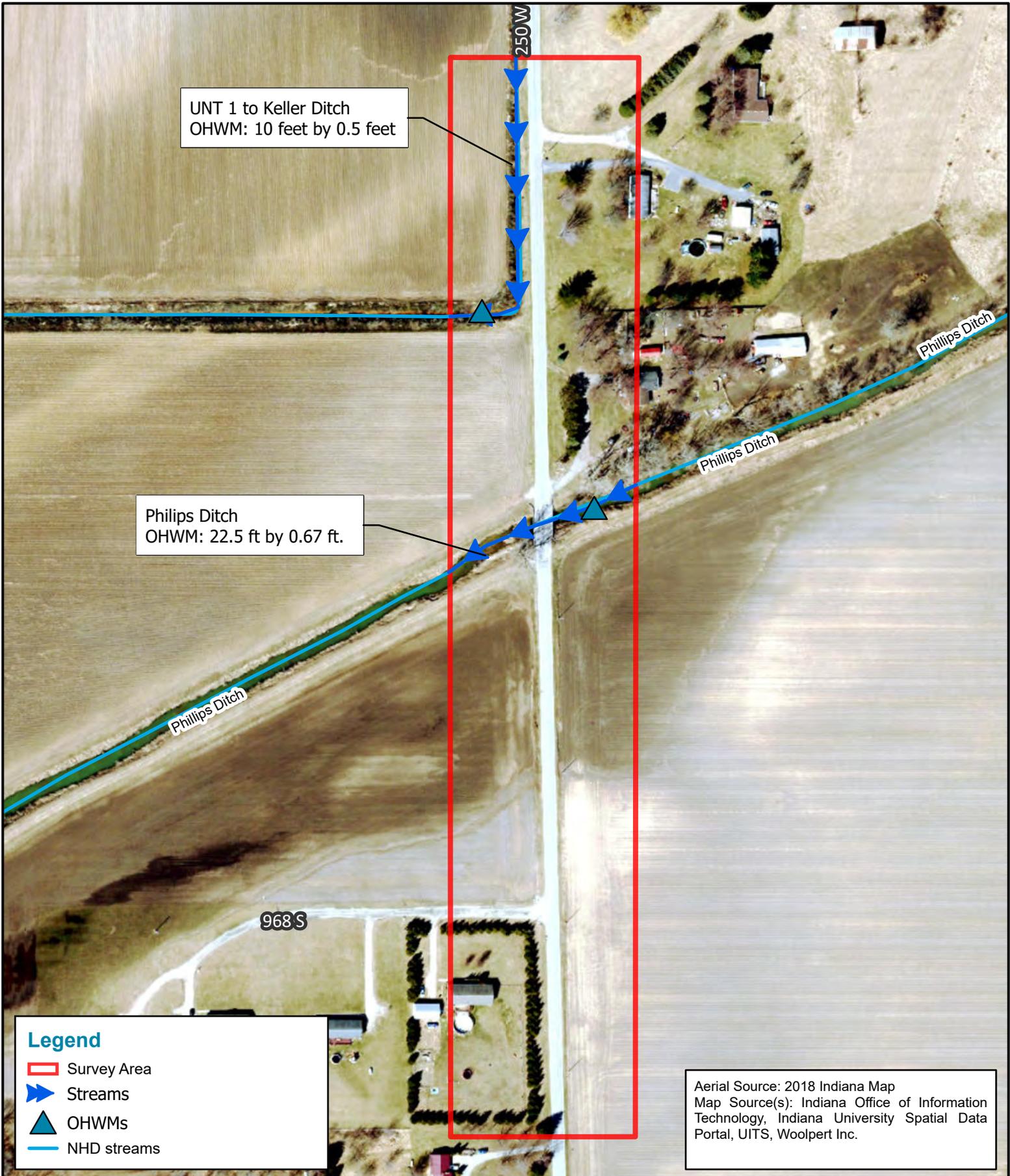
3502 Woodview Trace, Suite 150
 Indianapolis, IN 46268
 Phone: (317) 222-3880
 Fax: (317) 222-3881

BA Flood Hazard & NHD Line Map
 Des. No. 2003041
 Waters of the U.S. Report

0 150 300 Feet

County: Porter
 Township: Boone
 State: Indiana

Porter Co. Bridge 66
 Bridge Replacement
 Created: 8/31/2022, RHook



Legend

- ▭ Survey Area
- Streams
- ▲ OHWMs
- NHD streams

Aerial Source: 2018 Indiana Map
 Map Source(s): Indiana Office of Information Technology, Indiana University Spatial Data Portal, UITS, Woolpert Inc.

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Water Resources Map
 Des. No. 2003041
 Waters of the U.S. Report

0 100 200 Feet

County: Porter
 Township: Boone
 State: Indiana

Porter Co. Bridge 66
 Bridge Replacement
 Created: 8/31/2022, RHook

Appendix 2 - PRELIMINARY JURISDICTIONAL DETERMINATION (PJD) FORM

BACKGROUND INFORMATION

A. REPORT COMPLETION DATE FOR PJD: September 7, 2022

B. NAME AND ADDRESS OF PERSON REQUESTING PJD: Ruth Hook, 112 W. Jefferson Blvd, Suite 500, South Bend, IN 46601

C. DISTRICT OFFICE, FILE NAME, AND NUMBER:

D. PROJECT LOCATION(S) AND BACKGROUND INFORMATION:

The project is located along CR 250 W, 0.5 mile south of CR 900 South in Section 21, Township 33 N, Range 6 West, Boone Township, Porter County, Indiana. The proposed project (Des. No. 2003041) will involve replacement of the existing structure, (Bridge No. 64-00066, NBI No. 6400066). The existing structure is a single span pre-stressed concrete adjacent box-beam bridge with a total length of 52.5 feet. The replacement structure is expected to be a three-span concrete slab bridge or a single span pre-stressed concrete beam bridge.

(USE THE TABLE BELOW TO DOCUMENT MULTIPLE AQUATIC RESOURCES AND/OR AQUATIC RESOURCES AT DIFFERENT SITES)

State: **IN** County/parish/borough: **Porter** City: **N/A**

Center coordinates of site (lat/long in degree decimal format):

Lat.: **41.29551** Long.: **-87.112679**

Universal Transverse Mercator: 490565.97 E, 4571568.44 N Z 16T

Name of nearest waterbody: **Philips Ditch**

E. REVIEW PERFORMED FOR SITE EVALUATION (CHECK ALL THAT APPLY):

Office (Desk) Determination. Date:

Field Determination. Date(s):

TABLE OF AQUATIC RESOURCES IN REVIEW AREA WHICH “MAY BE” SUBJECT TO REGULATORY JURISDICTION.

Site number	Latitude (decimal degrees)	Longitude (decimal degrees)	Estimated amount of aquatic resource in review area (acreage and linear feet, if applicable)	Type of aquatic resource (i.e., wetland vs. non-wetland waters)	Geographic authority to which the aquatic resource “may be” subject (i.e., Section 404 or Section 10/404)
Philips Ditch	41.29550°N +	-87.11270°W	336 lft (0.17 ac)	Non-Wetland	Section 404
UNT 1 to Keller Ditch	41.29700°N +	-87.11280°W	514 lft (0.12 ac)	Non-Wetland	Section 404

- 1) The Corps of Engineers believes that there may be jurisdictional aquatic resources in the review area, and the requestor of this PJD is hereby advised of his or her option to request and obtain an approved JD (AJD) for that review area based on an informed decision after having discussed the various types of JDs and their characteristics and circumstances when they may be appropriate.
- 2) In any circumstance where a permit applicant obtains an individual permit, or a Nationwide General Permit (NWP) or other general permit verification requiring “pre-construction notification” (PCN), or requests verification for a non-reporting NWP or other general permit, and the permit applicant has not requested an AJD for the activity, the permit applicant is hereby made aware that: (1) the permit applicant has elected to seek a permit authorization based on a PJD, which does not make an official determination of jurisdictional aquatic resources; (2) the applicant has the option to request an AJD before accepting the terms and conditions of the permit authorization, and that basing a permit authorization on an AJD could possibly result in less compensatory mitigation being required or different special conditions; (3) the applicant has the right to request an individual permit rather than accepting the terms and conditions of the NWP or other general permit authorization; (4) the applicant can accept a permit authorization and thereby agree to comply with all the terms and conditions of that permit, including whatever mitigation requirements the Corps has determined to be necessary; (5) undertaking any activity in reliance upon the subject permit authorization without requesting an AJD constitutes the applicant’s acceptance of the use of the PJD; (6) accepting a permit authorization (e.g., signing a proffered individual permit) or undertaking any activity in reliance on any form of Corps permit authorization based on a PJD constitutes agreement that all aquatic resources in the review area affected in any way by that activity will be treated as jurisdictional, and waives any challenge to such jurisdiction in any administrative or judicial compliance or enforcement action, or in any administrative appeal or in any Federal court; and (7) whether the applicant elects to use either an AJD or a PJD, the JD will be processed as soon as practicable. Further, an AJD, a proffered individual permit (and all terms and conditions contained therein), or individual permit denial can be administratively appealed pursuant to 33 C.F.R. Part 331. If, during an administrative appeal, it becomes appropriate to make an official determination whether geographic jurisdiction exists over aquatic resources in the review area, or to provide an official delineation of jurisdictional aquatic resources in the review area, the Corps will provide an AJD to accomplish that result, as soon as is practicable. This PJD finds that there “*may be*” waters of the U.S. and/or that there “*may be*” navigable waters of the U.S. on the subject review area, and identifies all aquatic features in the review area that could be affected by the proposed activity, based on the following information:

SUPPORTING DATA. Data reviewed for PJD (check all that apply)

Checked items should be included in subject file. Appropriately reference sources below where indicated for all checked items:

- Maps, plans, plots or plat submitted by or on behalf of the PJD requestor:
Map: Location, USGS Topo, NRCS Soils, USFWS NWI, Floodplains, Water Resources.
- Data sheets prepared/submitted by or on behalf of the PJD requestor.
 - Office concurs with data sheets/delineation report.
 - Office does not concur with data sheets/delineation report. Rationale: _____.
- Data sheets prepared by the Corps: _____.
- Corps navigable waters' study: _____.
- U.S. Geological Survey Hydrologic Atlas: Hydrography_LocalRes_Flowline_Classified_NHD_IN.
 - USGS NHD data.
 - USGS 8 and 12 digit HUC maps.
- U.S. Geological Survey map(s). Cite scale & quad name: Kouts 1:24,000.
- Natural Resources Conservation Service Soil Survey. Citation: Porter Co. 2020.
- National wetlands inventory map(s). Cite name: Indiana NWI Geodatabase.
- State/local wetland inventory map(s): _____.
- FEMA/FIRM maps: DNR/FloodHazard_BestAvailable_IDNR_IN.
- 100-year Floodplain Elevation is: 655.70 (National Geodetic Vertical Datum of 1929).
- Photographs: Aerial (Name & Date): 2018 Porter Co (Indiana Map)
or Other (Name & Date): August 2022 Field Photos.
- Previous determination(s). File no. and date of response letter: _____.
- Other information (please specify): _____.

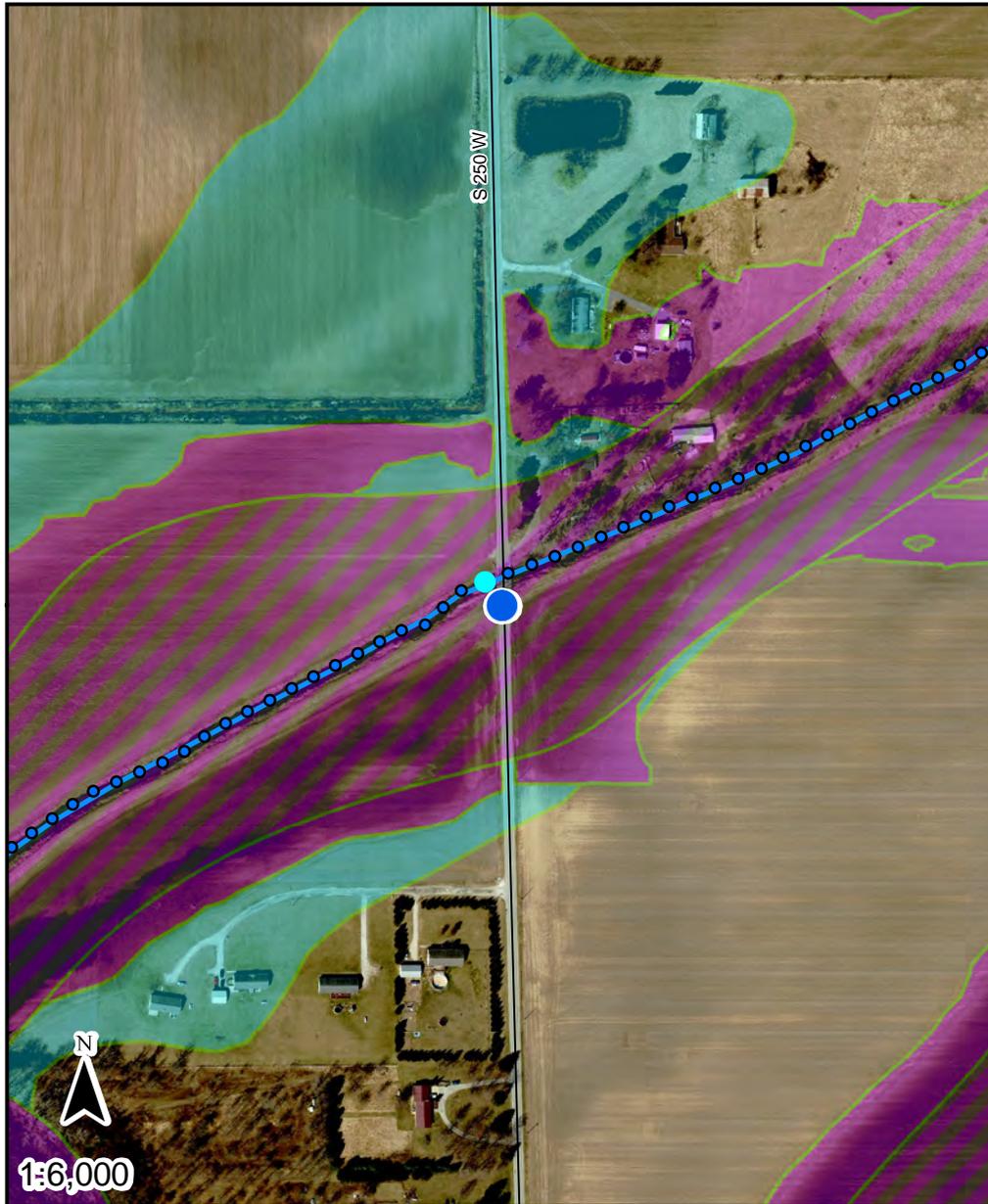
IMPORTANT NOTE: The information recorded on this form has not necessarily been verified by the Corps and should not be relied upon for later jurisdictional determinations.

Signature and date of
Regulatory staff member
completing PJD

Ruth Hook Digitally signed by Ruth Hook
Date: 2022.08.31 09:36:54 -04'00'

Signature and date of
person requesting PJD
(REQUIRED, unless obtaining
the signature is impracticable)¹

¹ Districts may establish timeframes for requestor to return signed PJD forms. If the requestor does not respond within the established time frame, the district may presume concurrence and no additional follow up is necessary prior to finalizing an action.



- Point of Interest
- Base Flood Elevation Point
- Flood Elevation Points**
 - STUDIED STREAM
- Rivers and Streams at least 1 square mile**
- Drainage Area (sq. miles)**
 - 10 - 100
- DNR Approximate Floodway
- FEMA Zone AE
- DNR Approximate Fringe

Point of Interest Coordinates (WGS84)
 Long: **-87.1126935662**
 Lat: **41.2953539988**

The information provided below is based on the point of interest shown in the map above.

County: **Porter**

Approximate Ground Elevation: **657.8 feet (NAVD88)**

Stream Name:
Phillips Ditch

Base Flood Elevation: **655.6 feet (NAVD88)**

Drainage Area: **Not available**

Best Available Flood Hazard Zone: **DNR Approximate Floodway**

National Flood Hazard Zone: **FEMA Zone AE**

Is a Flood Control Act permit from the DNR needed for this location? **yes**

Is a local floodplain permit needed for this location? **yes-**

Floodplain Administrator: **Bob Thompson, Executive Director, Plan Commission**

Community Jurisdiction: **Porter County, County proper**

Phone: **(219) 465-3540**

Email: **rthompson@porterco.org**

US Army Corps of Engineers District: **Chicago**

Date Generated: 12/14/2022

Categorical Exclusion
Appendix G
Public Involvement



SAMPLE NOTICE
OF SURVEY

February 25, 2022

NOTICE OF SURVEY

RE: Porter County Bridge 66 (C.R. 250W over Phillips Ditch) Replacement, 0.57 miles south of C.R. 900S, in Porter County, Indiana
Loch Group Project No.: 121-3037-01B
INDOT Des. #2003041.

Dear Property Owner:

Research of county records indicates that you own or occupy property(s) near this proposed bridge replacement project. Our employees will be doing a survey of the project area(s) in the near future. It may be necessary for them to come onto your property to complete this work. If you are available, our surveyors will show identification before coming onto your property. If you have sold this property, or it is occupied by someone else, please advise us of the name and address of the current owner/occupant so that we may contact them about the survey.

At this stage we do not know what effect, if any, our project may eventually have on your property. If we determine later that your property is involved, we will contact you with additional information.

The survey work will include mapping the location of features such as buildings, trees, fences and drives, as well as obtaining ground elevations. The survey work may include the identification and mapping of wetlands and streams, and various other environmental features. This work is necessary for the proper planning and design of this proposed bridge replacement project.

Please be assured of our sincere desire to cause you as little inconvenience as possible during this survey. If any problems do occur, please contact our field crew or call me at **(812-479-6200)**, email me at ssuttles@lochgroup.com or write to me at the above address. Thank you in advance for your cooperation.

Sincerely yours,

LOCHMUELLER GROUP, INC.

Sean L. Suttles, P.S.
Chief of Surveying

Categorical Exclusion
Appendix H
Air Quality

2003041 (Ver 1) 24-00 STATUS **New Project** **FEDERAL**

Title: Porter County Bridge 64-00066 - CR 250 W over Phillips Ditch **Route:** N/A
Description: Porter County Bridge 64-00066 will be replaced with a new bridge
Project Type: Bridge Replacement **AQ Exempt:** Exempt **District:** LaPorte
County: Porter **Limits:**
Narrative: Bridge 66 is proposed for replacement. Porter County is using local funds for PE, UT and RW. Federal funds are for CN and C

FED FY	REVENUE SOURCE	PE	RW	CN	CE	TOTAL
2022	Local Fund	\$350,000	\$0	\$0	\$0	\$350,000
2024	Local Fund	\$0	\$70,000	\$0	\$0	\$70,000
2026	Local Bridge	\$0	\$0	\$1,102,000	\$165,300	\$1,267,300
2026	Local Fund	\$0	\$0	\$275,500	\$41,325	\$316,825
2022-2026 TOTAL		\$350,000	\$70,000	\$1,377,500	\$206,625	\$2,004,125
ALL YEARS TOTAL		\$350,000	\$70,000	\$1,377,500	\$206,625	\$2,004,125

Region: Northwestern MPO

Lead Agency: Porter County

**Federal Transit
Administration**
Region V
200 West Adams St., Suite 320
Chicago, IL 60606-5253



**U.S. Department
of Transportation**

Federal Highway Administration
Indiana Division
575 N. Pennsylvania St., Rm 254
Indianapolis, IN 46204-1576

September 1, 2023

Mr. Michael Smith
Commissioner
Indiana Department of Transportation
100 N Senate Ave. N955
Indianapolis, IN 46204

SUBJECT: Indiana FY2024-2028 STIP Approval and Associated Federal Planning Finding

Dear Mr. Smith:

The Federal Highway Administration (FHWA) and the Federal Transit Administration (FTA) have completed our review of the FY2024-2028 Indiana Statewide Transportation Improvement Program (INSTIP), which was submitted by the Indiana Department of Transportation (INDOT) request letter dated August 23, 2023.

Based on our review of the information provided, certifications of the Statewide and Metropolitan transportation planning processes for and within the state of Indiana, and our participation in those transportation planning processes (including planning certification reviews conducted in Transportation Management Areas), FHWA and FTA are jointly approving the FY2024-2028 STIP, including the Metropolitan Planning Organization (MPO) Transportation Improvement Programs (TIPs) incorporated into the STIP by reference, subject to the corrective actions identified in the attached Federal Planning Finding (FPF) report. FHWA and FTA consider the projects in the 5th year for informational purposes only, and our approval does not exceed four years per 23 CFR 450.220(c).

FHWA and FTA are required under 23 CFR 450.220(b) to document and issue an FPF in conjunction with the approval of the FY2024-2028 STIP. At a minimum, the FPF verifies that the development of the STIP is consistent with the provisions of both the Statewide and Metropolitan transportation planning requirements. FHWA and FTA find that the Indiana FY2024-2028 STIP substantially meets the transportation planning requirements and are approving the STIP subject to the corrective actions outlined in the FPF. This approval is effective September 1, 2023 and is given with the understanding that an eligibility determination of individual projects for funding must be met, and INDOT must ensure the satisfaction of all administrative and statutory requirements, as well as address the corrective actions outlined in the attached report.

If you have questions or need additional information concerning our approval and the FPF, please contact Ms. Erica Tait of the FHWA Indiana Division at (317) 226-7481, or by email at erica.tait@dot.gov, or Mr. Tony Greep of the FTA Region 5 Office at (312) 353-1646, or by email at anthony.greep@dot.gov.

Sincerely,

**KELLEY
BROOKINS** Digitally signed by
KELLEY BROOKINS
Date: 2023.08.31
17:33:15 -05'00'

Kelley Brookins
Regional Administrator
FTA Region V

Sincerely,

**JERMAINE
R HANNON** Digitally signed by
JERMAINE R HANNON
Date: 2023.09.01
11:46:31 -04'00'

Jermaine R. Hannon
Division Administrator
FHWA Indiana Division



INDIANA DEPARTMENT OF TRANSPORTATION

100 North Senate Avenue
Room N758-Executive Office
Indianapolis, Indiana 46204

PHONE: (855) 463-6848

Eric Holcomb, Governor
Michael Smith, Commissioner

August 28, 2023

Mr. Jermaine R. Hannon, Division Administrator
FHWA Indiana Division
575 North Pennsylvania St., Room 254
Indianapolis, IN 46204

Ms. Kelley Brookins, Regional Administrator
FTA Region 5
200 West Adams St.
Suite 320
Chicago, IL 60606-5253

Dear Mr. Hannon /Ms. Brookins:

The Indiana Department of Transportation is pleased to submit its FY 2024-2028 Statewide Transportation Improvement Program (STIP) for review and approval by your offices.

Included in the final submitted document is a listing of the state’s expansion/preservation and local small urban and rural and rural transit projects. The following Metropolitan Planning Organization TIPs will be included in the FY 2024-2028 STIP by reference.

- | | |
|---|--------------|
| Area Plan Commission of Tippecanoe County (APCTC) | FY 2024-2028 |
| • https://www.tippecanoe.in.gov/DocumentCenter/View/40728/FY-2024-2028-TIP-including-0-amendments | |
| Bloomington-Monroe County Metropolitan Planning Organization (BMCMPPO) | FY 2024-2028 |
| • https://bloomington.in.gov/sites/default/files/2023-08/BMCMPPO%20FY%202024%20-%202028%20TIP%20-%202006-30-23%20-%20ADOPTED%20FINAL.pdf | |
| Columbus Area Metropolitan Planning Organization (CAMPO) | FY 2024-2028 |
| • https://www.columbus.in.gov/planning/tip/ | |
| Delaware-Muncie Metropolitan Plan Commission (DMMPC) | FY 2022-2025 |
| • <i>Including Amendments/modifications through 2/14/23</i> | |
| • https://www.co.delaware.in.us/egov/documents/1692987897_47263.pdf | |
| Evansville Metropolitan Planning Organization (EMPO) | FY 2024-2028 |
| • http://www.evansvillempo.com/Docs/TIP/TIP_2024-2028/TIP_2024-2028.pdf | |
| Kokomo-Howard County Governmental Coordinating Council (KHCGCC) | FY 2022-2026 |
| • <i>Including Amendments/modification through 7/28/23</i> | |
| • https://www.kokomompo.com/project/tip-2020-2024/ | |

www.in.gov/dot/

An Equal Opportunity Employer



Kentuckiana Regional Planning and Development Agency (KIPDA)	FY 2023-2026
<ul style="list-style-type: none"> • https://www.kipda.org/wp-content/uploads/2023/05/FY2023-TIP-FINAL-5-25.pdf 	
Indianapolis Metropolitan Planning Organization (IMPO)	FY 2024-2027
<ul style="list-style-type: none"> • https://www.indympo.org/whats-underway/irtip 	
Michiana Area Council of Governments (MACOG)	FY 2024-2028
<ul style="list-style-type: none"> • http://www.macog.com/docs/transportation/tip/approved/fy2028tip_projects.pdf 	
Madison County Council of Governments (MCCOG)	FY 2022-2026
<ul style="list-style-type: none"> • <i>Including Amendments/modifications through 7/28/23</i> • https://irp.cdn-website.com/65a760a0/files/uploaded/TIP%202022-2026%20-%20updated%205-1-23.pdf 	
Northeastern Indiana Regional Coordinating Council (NIRCC)	FY 2024-2028
<ul style="list-style-type: none"> • https://www.nircc.com/uploads/1/2/9/8/129837621/final_2024-2028_tip_5-25-23.pdf 	
Northwestern Indiana Regional Planning Commission (NIRPC)	FY 2022-2026
<ul style="list-style-type: none"> • <i>Including Amendments/modifications through 7/25/23</i> • https://nirpc.org/2040-plan/mobility/transportation-improvement-program/ 	
Ohio-Kentucky-Indiana Regional Council of Governments (OKI)	FY 2024-2027
<ul style="list-style-type: none"> • https://www.oki.org/transportation-planning/transportation-improvement-program-tip/ 	
Terre Haute Area Metropolitan Planning Organization (THAMPO)	FY 2024-2028
<ul style="list-style-type: none"> • https://www.terrehautempo.com/images/THAMPO_2024_2028_AdoptionTIP.pdf 	

In addition, INDOT has expanded our public involvement process by taking advantage of virtual meeting techniques and allowing accessibility to online documents, materials, virtual meeting registration, recorded virtual meetings, and comment forms. INDOT also leveraged our planning partner contacts (MPOs, RPOs, LTAP), social media, and notifications sent to local libraries, housing authorities, senior aging centers, and local newspapers across the state.

We greatly appreciate FHWA/FTA support in the development of the STIP 2024-2028 and look forward to working together to achieve our mutual goals. Should you have any questions pertaining to this amendment, please contact April Leckie, STIP Administration at 317-232-5466 or at aleckie@indot.in.gov.

Sincerely,



Michael Smith, Commissioner
Indiana Department of Transportation

cc: (w/enclosure): Angelica Salgado, FTA
Cecilia Crenshaw, FTA
Erica Tait, FHWA
Lyndsay Quist, INDOT
Kristin Brier, INDOT
Kathy Eaton-McKalip, INDOT
Louis Feagans, INDOT

April Leckie, INDOT
Roy Nunnally, INDOT
Larry Buckel, INDOT
Jay Mitchell, INDOT
Jason Casteel, INDOT
Michael McNeil, INDOT

Categorical Exclusion
Appendix I
Other

Land and Water Conservation Fund (LWCF) County Property List for Indiana (Last Updated March 2022)

ProjectNumber	SubProjectCode	County	Property
1800037	1800037	Porter	Woodland Park
1800050	1800050	Porter	Forest Park Golf Course
1800065	1800065	Porter	Woodland Park
1800080	1800080	Porter	Woodland Park
1800127	1800127	Porter	Indiana Dunes State Park & Dunes Nature Preserve
1800130	1800130	Porter	Bicentennial Park, Northside Park
1800171	1800171E	Porter	Indiana Dunes State Park
1800173	1800173	Porter	Indiana Dunes State Park & Dunes Nature Preserve
1800270	1800270	Porter	Harold Rogers Lakewood Park
1800276	1800276	Porter	Imagination Glen Park
1800284	1800284	Porter	Dogwood Park
1800304	1800304B	Porter	Moraine
1800312	1800312E	Porter	Indiana Dunes State Park
1800327	1800327D	Porter	Indiana Dunes State Park
1800349	1800349	Porter	Thomas Drazer Memorial Park & Pleasant Twp. Pool
1800363	1800363M	Porter	Indiana Dunes State Park
1800378	1800378B	Porter	Indiana Dunes State Park
1800405	1800405Q	Porter	Moraine Nature Preserve
1800407	1800407	Porter	Imagination Glen Park
1800413	1800413L	Porter	Indiana Dunes State Park
1800429	1800429	Porter	Indiana Dunes State Park & Dunes Nature Preserve
1800443	1800443	Porter	Haven Hollow Park
1800452	1800452	Porter	Sunset Hill Farm County Park
1800460	1800460	Porter	Indiana Dunes State Park & Dunes Nature Preserve
1800484	1800484	Porter	Prairie Duneland Trail
1800495	1800495	Porter	Prairie Duneland Trail
1800498	1800498	Porter	Prairie Duneland Trail
1800539	1800539	Porter	Imagination Glen Park
1800591	1800591	Porter	Sunset Hill Farm Park
1800624	1800624	Porter	Imagination Glen Park
1800626	1800626C	Porter	Indiana Dunes State Park

*Park names may have changed. If acquisition of publically owned land or impacts to publically owned land is anticipated, coordination with IDNR, Division of Outdoor Recreation, should occur.

Bridge Inspection Report

64-00066
CR 250W
over
PHILLIPS DITCH



Inspection Date: 5/23/2022

Inspected By: Stuart May

Inspection Type(s): Routine

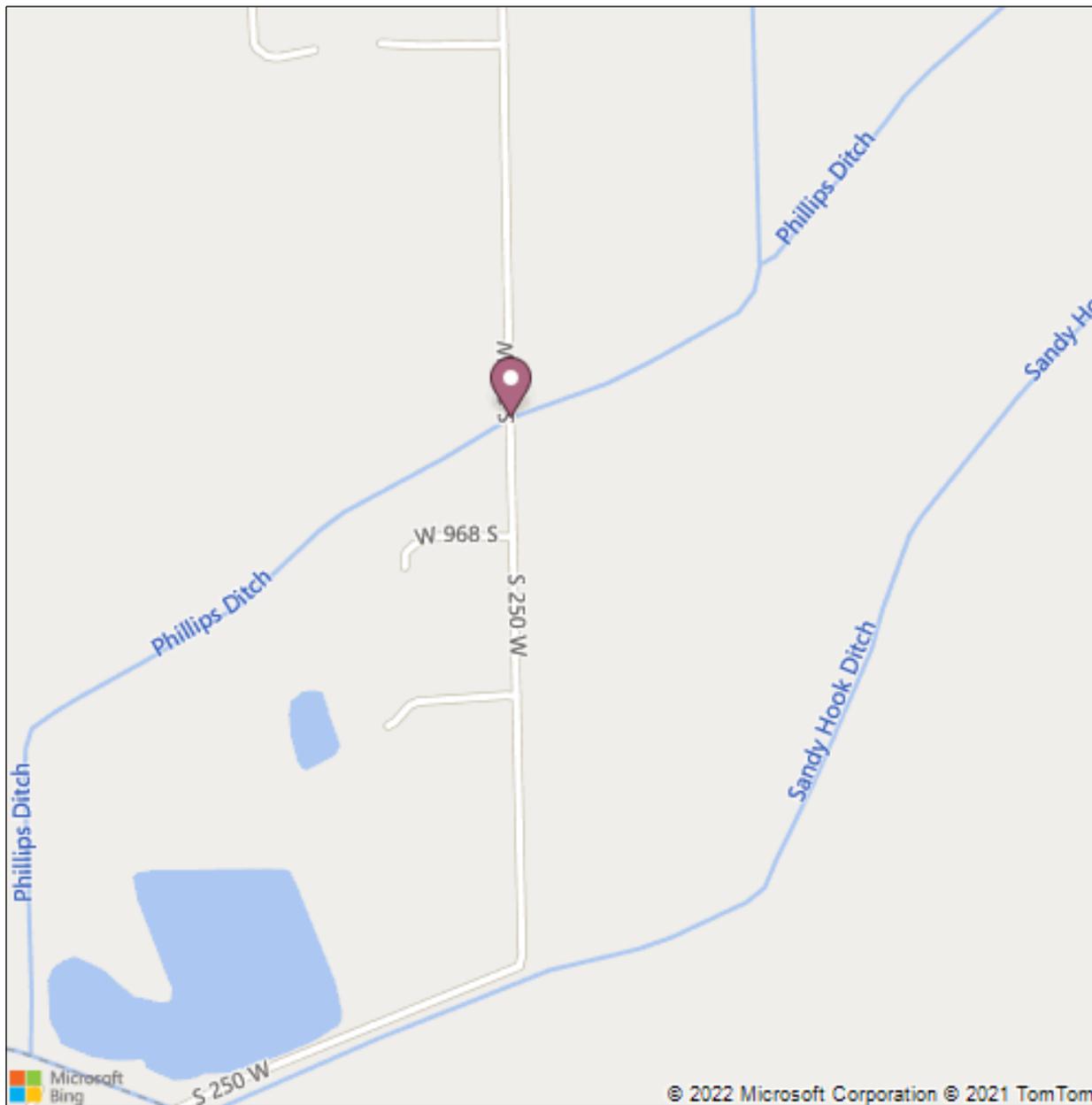
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Inspector: Stuart May
Inspection Date: 5/23/2022

Asset Name: 64-00066
Facility Carried: CR 250W

Bridge Inspection Report



Latitude: 41.29551
Longitude: -87.11267

Inspector: Stuart May
Inspection Date: 5/23/2022

Asset Name: 64-00066
Facility Carried: CR 250W

Bridge Inspection Report

The bridge was built in 1930 and reconstructed in 1996.

Wearing Surface - LONGITUDINAL CRACKS IN WEARING SURFACE ALONG BEAM EDGES. TRANSVERSE CRACKS AT BRIDGE ENDS, HEAVY MAP CRACKING THROUGHOUT, PATCHES INTERMITTENT.

Deck - APPEAR WIDENED WITH NEWER BEAMS AT COPINGS AND TIE RODS PLACED ON UNDERSIDE AT 1/3 POINTS.

Superstructure - APPEAR WIDENED WITH NEWER BEAMS AT COPINGS AND TIE RODS PLACED ON UNDERSIDE AT 1/3 POINTS, EXTERIOR BEAMS HAVE MORE CAMBER THAN ADJACENT BEAM, BEAM 8 BEARING ON CORNER AND SHEET PILING, EFFLORESCENCE AT BEAM JOINTS.

Substructure - WIDENED @ EAST ABUTMENT ENDS WITH CONCRETE AND STEEL SHEET PILE, NORTH ABUTMENT DETERIORATION APPROXIMATELY 0.5" WIDE VERTICAL CRACK AND FOOTING WITH WEST HALF SLIGHTLY OFFSET TOWARD THE CHANNEL, SPALLING UP TO 6" DEEP ALONG NORTH ABUTMENT FACE.

Channel - SILT FLOWLINE, NATURAL BANKS, SILT BUILDUP ON SOUTH SPILLSLOPE AND AT NORTHEAST CORNER, RIPRAP INSTALLED AT CORNERS, RIPRAP NOT INSTALLED ON SPILLSLOPES, CHANNEL EMBANKMENT EROSION..

The bridge is scour critical.

Overall the bridge is in poor condition.

Inspector: Stuart May
 Inspection Date: 5/23/2022

Asset Name: 64-00066
 Facility Carried: CR 250W

Bridge Inspection Report

IDENTIFICATION

(1) STATE CODE:	185 - Indiana	(12) BASE HIGHWAY NETWORK:	0
(8) STRUCTURE:	6400044	(13A) INVENTORY ROUTE:	
(5 A-B-C-D-E) INV. ROUTE:	1 - 4 - 1 - 00029 - 0	(13B) SUBROUTE NUMBER:	
(2) HIGHWAY AGENCY DISTRICT:	04 - La Porte	(16) LATITUDE:	41.29551
(3) COUNTY CODE:	064 - PORTER	(17) LONGITUDE:	-87.11267
(4) PLACE CODE:	00000 - N/A	(98) BORDER	
(6) FEATURES INTERSECTED:	PHILLIPS DITCH	A) STATE NAME:	
(7) FACILITY CARRIED:	CR 250W	B) PERCENT	%
(9) LOCATION:	00.50 S CR 900 S	(99) BORDER BRIDGE STRUCT. NO:	
(11) MILEPOINT:	0000.000		

STRUCTURE TYPE AND MATERIAL

(43) STRUCTURE TYPE, MAIN:		(45) NUMBER OF SPANS IN MAIN 001 UNIT:	
A) KIND OF MATERIAL/DESIGN:	5 - Prestressed concrete	(46) NUMBER OF APPROACH SPANS:	0
B) TYPE OF DESIGN/CONSTR:	05 - Box Beam or Girders - Multiple	(107) DECK STRUCTURE TYPE:	2 - Concrete Precast Panels
(44) STRUCTURE TYPE, APPROACH SPANS:		(108) WEARING SURFACE/PROT SYS:	
A) KIND OF MATERIAL/DESIGN:	0 - Other	A) WEARING SURFACE:	6 - Bituminous
B) TYPE OF DESIGN/CONSTR:	00 - Other	B) DECK MEMBRANE:	0 - None
		C) DECK PROTECTION:	0 - None

AGE OF SERVICE

(27) YEAR BUILT:	1930	(28) LANES:	
(106) YEAR RECONSTRUCTED:	1996	A) ON BRIDGE:	02
(42) TYPE OF SERVICE:		B) UNDER BRIDGE:	00
A) ON BRIDGE:	1 - Highway	(29) AVERAGE DAILY TRAFFIC:	000640
B) UNDER BRIDGE:	5 - Water way	(30) YEAR OF AVERAGE DAILY TRAFFIC:	2018
		(109) AVERAGE DAILY TRUCK TRAFFIC:	06 %
		(19) BYPASS DETOUR LENGTH:	014 MI

Bridge Inspection Report

GEOMETRIC DATA

(48) LENGTH OF MAX SPAN: 00050.0 FT	(35) STRUCTURE FLARED: 0 - No flare
(49) STRUCTURE LENGTH: 00052.5 FT	(10) INV RTE, MIN VERT CLEARANCE: 99.99 FT
(50) CURB/SIDEWALK WIDTHS:	(47) TOT HORIZ CLEARANCE: 030.0 FT
A) LEFT 00.0 FT	(53) VERT CLEAR OVER BR RDWY: 99.99 FT
B) RIGHT: 00.0 FT	(54) MIN VERTICAL UNDERCLEARANCE:
(51) BRDG RDWY WIDTH CURB-TO-CURB: 030.0 FT	A) REFERENCE FEATURE: N
(52) DECK WIDTH, OUT-TO-OUT: 030.6 FT	B) MIN VERT UNDERCLEAR: 00.00 FT
(32) APPROACH ROADWAY 020.0 FT	(55) LATERAL UNDERCLEARANCE RIGHT:
(33) BRIDGE MEDIAN: 0 - No median	A) REFERENCE FEATURE: N
(34) SKEW: 25 DEG	B) MIN LATERAL UNDERCLEAR: 000.0 FT
	(56) MIN LATERAL UNDERCLEAR ON LEFT: 000.0 FT

INSPECTIONS

(90) INSPECTION DATE: 05/23/2022	(91) DESIGNATED INSPECTION FREQUENCY: 24 MONTHS
(92) CRITICAL FEATURE INSPECTION:	(93) CRITICAL FEATURE INSPECTION DATE:
A) FRACTURE CRITICAL REQUIRED/FREQUENCY: N	A) FRACTURE CRITICAL DATE:
B) UNDERWATER INSPECTION REQUIRED/FREQUENCY: N	B) UNDERWATER INSP DATE:
C) OTHER SPECIAL INSPECTION REQUIRED/FREQUENCY: N	C) OTHER SPECIAL INSP DATE:

CONDITION

(58) DECK: 6 - Satisfactory Condition (minor deterioration)	(60) SUBSTRUCTURE: 5 - Fair Condition (minor section loss)
(58.01) WEARING SURFACE: 5 - Fair Condition	(61) CHANNEL/CHANNEL PROTECTION: 5 - Bank eroded.. major damage
(59) SUPERSTRUCTURE: 6 - Satisfactory Condition (minor deterioration)	(62) CULVERTS: N - Not Applicable

CONDITION COMMENTS

(58) DECK: 6 - Satisfactory Condition (minor deterioration)

Comments:
 SATIS - MODERATE SEEPAGE WITH EFFLORESCENCE BETWEEN BEAMS
 Material:
 PRESTRESSED CONCRETE BOX BEAMS (21")

(58.01) WEARING SURFACE: 5 - Fair Condition

Comments:
 FAIR - LONGITUDINAL CRACKS IN WEARING SURFACE ALONG BEAM EDGES. TRANSVERSE CRACKS AT BRIDGE ENDS, HEAVY MAP CRACKING THROUGHOUT, PATCHES INTERMITTENT.
 Material:
 BITUMINOUS (3")

Inspector: Stuart May
 Inspection Date: 5/23/2022

Asset Name: 64-00066
 Facility Carried: CR 250W

Bridge Inspection Report

(59) SUPERSTRUCTURE: 6 - Satisfactory Condition (minor deterioration)

Comments:

SATIS - WIDENED WITH NEWER BEAMS AT COPINGS AND TIE RODS PLACED ON UNDERSIDE AT 1/3 POINTS, EXTERIOR BEAMS HAVE MORE CAMBER THAN ADJACENT BEAM, BEAM 8 BEARING ON CORNER AND SHEET PILING, MODERATE EFFLORESCENCE AT BEAM JOINTS

Material:

PRESTRESSED CONCRETE BOX BEAMS (21")

(60) SUBSTRUCTURE: 5 - Fair Condition (minor section loss)

Comments:

FAIR - WIDENED @ EAST ABUTMENT ENDS WITH CONCRETE AND STEEL SHEET PILE, NORTH ABUTMENT DETERIORATION APPROXIMATELY 0.5" WIDE VERTICAL CRACK AND FOOTING WITH WEST HALF SLIGHTLY OFFSET TOWARD THE CHANNEL, SPALLING UP TO 6" DEEP ALONG NORTH ABUTMENT FACE

Material:

CONCRETE ABUTMENTS

(61) CHANNEL/CHANNEL PROTECTION: 5 - Bank eroded.. major damage

Comments:

FAIR - SILT FLOWLINE, NATURAL BANKS, SILT BUILDUP ON SOUTH SPILLSLOPE AND AT NORTHEAST CORNER, NEW RIPRAP INSTALLED ALONG NORTH ABUTMENT PROTECTING FOOTING, NO PROTECTION ALONG SOUTH ABUTMENT, CHANNEL EMBANKMENT EROSION

Material:

NATURAL BANKS

(62) CULVERTS: N - Not Applicable

Comments:

N/A

Material:

N/A

LOAD RATING AND POSTING

(31) DESIGN LOAD:	5 - HS 20	(66) INVENTORY RATING:	0.606
(70) BRIDGE POSTING	5 - Equal to or above legal loads	(65) INVENTORY RATING METHOD:	8 - Load and Resistance Factor Rating (LRFR) rating report by rating factor (RF) method using HL-93 loadings.
(41) STRUCTURE OPEN/POSTED/CLOSED:	A - Open	(66B) INVENTORY RATING (H):	
(64) OPERATING RATING:	0.785	(66C) TONS POSTED :	
(63) OPERATING RATING METHOD:	8 - Load and Resistance Factor Rating (LRFR) rating report by rating factor (RF) method using HL-93 loadings.	(66D) DATE POSTED/CLOSED:	

APPRAISAL

SUFFICIENCY RATING:	70.4	(36) TRAFFIC SAFETY FEATURE:	
STATUS:	0	36A) BRIDGE RAILINGS:	0
(67) STRUCTURAL EVALUATION:	5	36B) TRANSITIONS:	0
(68) DECK GEOMETRY:	6	36C) APPROACH GUARDRAIL:	0
(69) UNDERCLEARANCES, VERTICAL & HORIZONTAL:	N	36D) APPROACH GUARDRAIL ENDS:	0

Inspector: Stuart May
 Inspection Date: 5/23/2022

Asset Name: 64-00066
 Facility Carried: CR 250W

Bridge Inspection Report

- (71) WATERWAY ADEQUACY: 6 - Occasional Overtopping of Approaches - Insignificant Delays
 Comments:
 ADEQUATE
- (72) APPROACH ROADWAY ALIGNMENT: 8 - Equal to present desirable criteria
 Comments:
 BITUMINOUS APPROACHES DETERIORATED AND SAGGING.
 Material:
 BITUMINOUS
 72: STRAIGHT, LEVEL, HILL SOUTH, DRIVES AT CORNERS
- (113) SCOUR CRITICAL BRIDGES: 5 - Scour within limits of footing or piles
 Comments:
 NEW COUNTERMEASURES PLACED TO PROTECT NORTH ABUTMENT

CLASSIFICATION

(20) TOLL:	3 - On Free Road	(21) MAINT. RESPONSIBILITY:	02 - County Highway Agency
(22) OWNER:	02 - County Highway Agency	(26) FUNCTIONAL CLASS OF INVENTORY RTE:	07 - Rural - Major Collector
(37) HISTORICAL SIGNIFICANCE:	5 - Not eligible	(100) STRAHNET HIGHWAY:	Not a STRAHNET route
(101) PARALLEL STRUCTURE:	N - No parallel structure	(102) DIRECTION OF TRAFFIC:	2-way traffic
(103) TEMPORARY STRUCTURE:		(104) HIGHWAY SYSTEM OF INVENTORY ROUTE:	0 - Structure/Route is NOT on NHS
(105) FEDERAL LANDS HIGHWAYS:	0-Not Applicable	(110) DESIGNATED NATIONAL NETWORK:	Inventory route not on network
(112) NBIS BRIDGE LENGTH:	Yes		

NAVIGATION DATA

(38) NAVIGATION CONTROL:	0 - No navigation control on waterway (bridge permit not required)	(39) NAVIGATION VERTICAL CLEAR:	000.0 FT
(111) PIER OR ABUTMENT PROTECTION:		(116) MINIMUM NAVIGATION VERT. CLEARANCE, VERT. LIFT BRIDGE:	FT
		(40) NAV HORIZONTAL CLEARANCE:	0000.0 FT

PROPOSED IMPROVEMENTS

(75A) TYPE OF WORK:	31 - Replacement - Load/Geometry	(95) ROADWAY IMPROVEMENT COST:	\$ 000287
(75B) WORK DONE BY:	1 - Work to be done by contract	(96) TOTAL PROJECT COST:	\$ 001395
(76) LENGTH OF IMPROVEMENT:	000075 FT	(97) YR OF IMPROVEMENT COST EST:	2022
(94) BRIDGE IMPROVEMENT COST:	\$ 000875	(114) FUTURE AVG DAILY TRAFFIC:	001210
		(115) YR OF FUTURE ADT:	2038

Inspector: May,Stuart
Inspection Date: 05/23/2022

Structure Number: 6400044
Facility Carried: CR 250W

Bridge Inspection Report

Approach Slabs: * Indicate if present & condition rating.

Comments:

Paint: * Indicate if paint present , year painted & condition rating.

N

Comments:

NOT APPLICABLE

Endangered Species: * If yes, add one photo to the dropdown field

Bats: seen or heard under structure? *

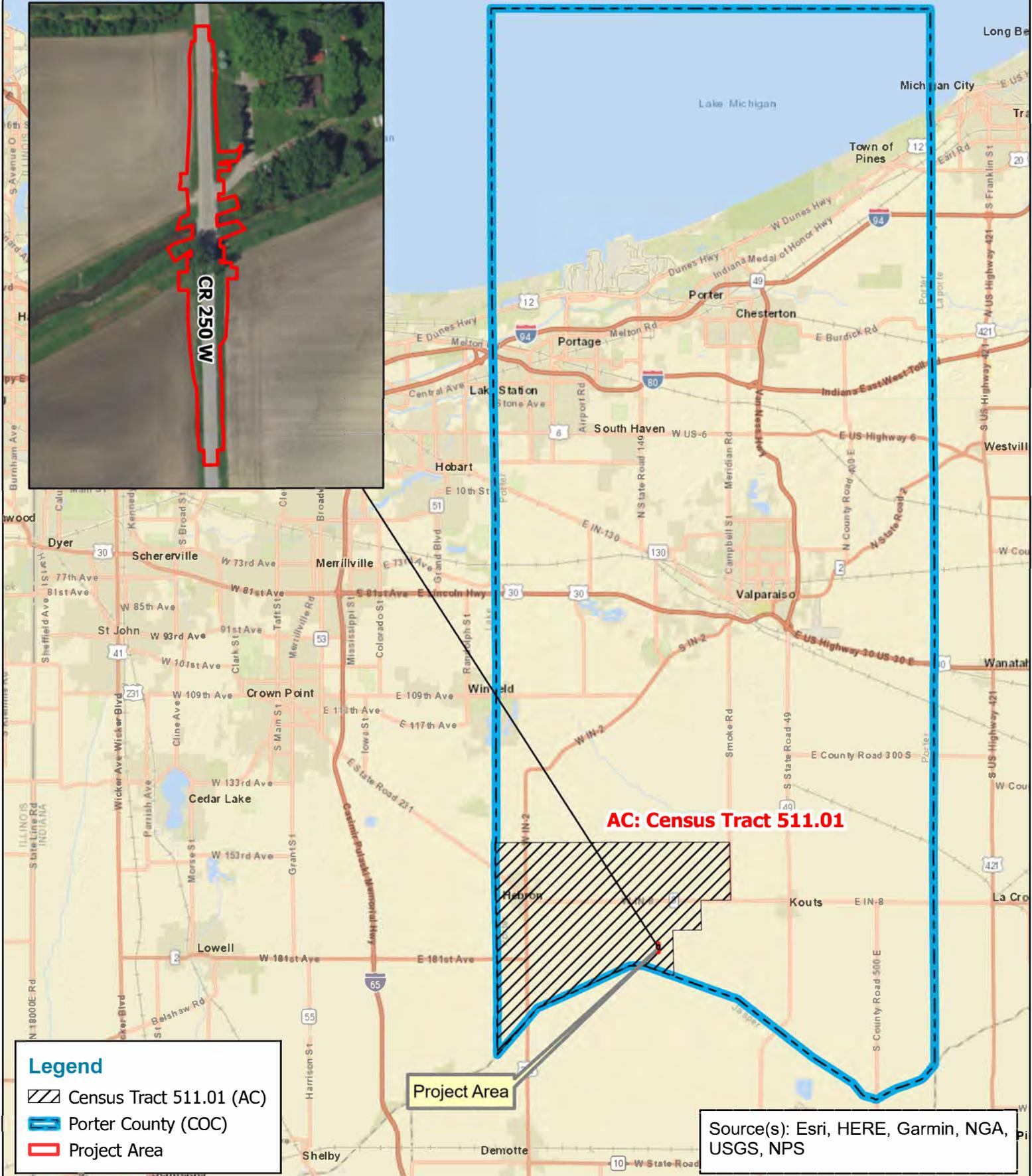
Birds/swallows/nests seen? Empty nests present? *

BRIDGE Culvert Geometry:

Barrel Length:

Height:

Width:



Legend

- Census Tract 511.01 (AC)
- Porter County (COC)
- Project Area

Source(s): Esri, HERE, Garmin, NGA, USGS, NPS

112 West Jefferson Blvd., Suite 500
 South Bend, IN 46601
 Phone: (574) 334-5460

EJ Location Map
 Des. No. 2003041
 Waters of the U.S. Report

0 3.75 7.5
 Miles

N

County: Porter
 Township: Boone
 State: Indiana

Porter Co. Bridge 66
 Bridge Project
 Created: 2/10/2023, CHoogewerf

Environmental Justice Calculations

	COC	AC 1
	Porter County	Census Tract 511.01
LOW-INCOME POPULATION		
Total Population for Whom Poverty Status is Determined	168,354	6,085
Total Population Below Poverty Level	17,017	379
Percent Low-Income	10.1%	6.2%
125 Percent of COC	12.6%	
AC Percent Low-Income Greater Than 125 Percent of COC?		NO
AC Percent Low-Income Greater Than 50 Percent?		NO
Population of EJ Concern?		NO
MINORITY POPULATION		
Total Population	172,353	6,105
Minority Population	31,569	503
Percent Minority	18.3%	8.2%
125 Percent of COC	22.9%	
AC Percent Minority Greater Than 125 Percent of COC?		NO
AC Percent Minority Greater Than 50 Percent?		NO
Population of EJ Concern?		NO

HISPANIC OR LATINO ORIGIN BY RACE		
Note: The table shown may have been modified by user selections. Some information may be missing.		
DATA NOTES		
TABLE ID:	B03002	
SURVEY/PROGRAM:	American Community Survey	
VINTAGE:	2021	
DATASET:	ACSDT5Y2021	
PRODUCT:	ACS 5-Year Estimates Detailed Tables	
UNIVERSE:	Total population	
FTP URL:	None	
API URL:	https://api.census.gov/data/2021/acs/acs5	
USER SELECTIONS		
TABLES	B03002	
GEOS	United States; Porter County, Indiana; Census Tract 511.01, Porter County, Indiana	
EXCLUDED COLUMNS		
	None	
APPLIED FILTERS		
	None	
APPLIED SORTS		
	None	
PIVOT & GROUPING		
PIVOT COLUMNS	None	
PIVOT MODE	Off	
ROW GROUPS	None	
VALUE COLUMNS	None	
WEB ADDRESS		
	https://data.census.gov/table?q=B03002:+HISPANIC+OR+LATINO+ORIGIN+BY+RACE&g=0100000US_0500000US18127_1400000US18127051101&tid=ACSDT5Y2021.B03002	
TABLE NOTES		
	Although the American Community Survey (ACS) produces population, demographic and housing unit estimates, it is the Census Bureau's Population Estimates Program that produces and disseminates the official estimates of the population for the nation, states, counties, cities, and towns and estimates of housing units for states and counties.	
	Supporting documentation on code lists, subject definitions, data accuracy, and statistical testing can be found on the American Community Survey website in the Technical Documentation section.	
	Sample size and data quality measures (including coverage rates, allocation rates, and response rates) can be found on the American Community Survey website in the Methodology section.	
	Source: U.S. Census Bureau, 2017-2021 American Community Survey 5-Year Estimates	
	Data are based on a sample and are subject to sampling variability. The degree of uncertainty for an estimate arising from sampling variability is represented through the use of a margin of error. The value shown here is the 90 percent margin of error. The margin of error can be interpreted roughly as providing a 90 percent probability that the interval defined by the estimate minus the margin of error and the estimate plus the margin of error (the lower and upper confidence bounds) contains the true value. In addition to sampling variability, the ACS estimates are subject to nonsampling error (for a discussion of nonsampling variability, see ACS Technical Documentation). The effect of nonsampling error is not represented in these tables.	
	The Hispanic origin and race codes were updated in 2020. For more information on the Hispanic origin and race code changes, please visit the American Community Survey Technical Documentation website.	

	<p>The 2017-2021 American Community Survey (ACS) data generally reflect the March 2020 Office of Management and Budget (OMB) delineations of metropolitan and micropolitan statistical areas. In certain instances, the names, codes, and boundaries of the principal cities shown in ACS tables may differ from the OMB delineation lists due to differences in the effective dates of the geographic entities.</p>
	<p>Estimates of urban and rural populations, housing units, and characteristics reflect boundaries of urban areas defined based on Census 2010 data. As a result, data for urban and rural areas from the ACS do not necessarily reflect the results of ongoing urbanization.</p>
	<p>Explanation of Symbols:- The estimate could not be computed because there were an insufficient number of sample observations. For a ratio of medians estimate, one or both of the median estimates falls in the lowest interval or highest interval of an open-ended distribution. For a 5-year median estimate, the margin of error associated with a median was larger than the median itself.N The estimate or margin of error cannot be displayed because there were an insufficient number of sample cases in the selected geographic area. (X) The estimate or margin of error is not applicable or not available.median- The median falls in the lowest interval of an open-ended distribution (for example "2,500-")median+ The median falls in the highest interval of an open-ended distribution (for example "250,000+").** The margin of error could not be computed because there were an insufficient number of sample observations.*** The margin of error could not be computed because the median falls in the lowest interval or highest interval of an open-ended distribution.***** A margin of error is not appropriate because the corresponding estimate is controlled to an independent population or housing estimate. Effectively, the corresponding estimate has no sampling error and the margin of error may be treated as zero.</p>
<p>COLUMN NOTES</p>	<p>None</p>

Table: ACSDT5Y2021.B03002

Label	Porter County, Indiana		Census Tract 511.01, Porter County, Indiana	
	Estimate	Margin of Error	Estimate	Margin of Error
Total:	172,353	*****	6,105	±18
Not Hispanic or Latino:	154,270	*****	5,669	±241
White alone	140,784	±615	5,602	±255
Black or African American alone	6,566	±351	0	±17
American Indian and Alaska Native alone	241	±95	5	±12
Asian alone	2,376	±250	22	±30
Native Hawaiian and Other Pacific Islander alone	25	±31	0	±17
Some other race alone	124	±94	0	±17
Two or more races:	4,154	±687	40	±38
Two races including Some other race	1,071	±610	13	±20
Two races excluding Some other race, and three or more races	3,083	±438	27	±33
Hispanic or Latino:	18,083	*****	436	±240
White alone	11,054	±864	333	±222
Black or African American alone	425	±343	0	±17
American Indian and Alaska Native alone	17	±18	0	±17
Asian alone	35	±37	0	±17
Native Hawaiian and Other Pacific Islander alone	0	±29	0	±17
Some other race alone	2,464	±599	75	±89
Two or more races:	4,088	±714	28	±45
Two races including Some other race	3,582	±700	0	±17
Two races excluding Some other race, and three or more races	506	±204	28	±45

POVERTY STATUS IN THE PAST 12 MONTHS BY SEX BY AGE		
Note: The table shown may have been modified by user selections. Some information may be missing.		
DATA NOTES		
TABLE ID:	B17001	
SURVEY/PROGRAM:	American Community Survey	
VINTAGE:	2021	
DATASET:	ACSDT5Y2021	
PRODUCT:	ACS 5-Year Estimates Detailed Tables	
UNIVERSE:	Population for whom poverty status is determined	
FTP URL:	None	
API URL:	https://api.census.gov/data/2021/acs/acs5	
USER SELECTIONS		
TABLES	B17001	
GEOS	United States; Porter County, Indiana; Census Tract 511.01, Porter County, Indiana	
EXCLUDED COLUMNS		
	None	
APPLIED FILTERS		
	None	
APPLIED SORTS		
	None	
PIVOT & GROUPING		
PIVOT COLUMNS	None	
PIVOT MODE	Off	
ROW GROUPS	None	
VALUE COLUMNS	None	
WEB ADDRESS		
	https://data.census.gov/table?q=B17001:+POVERTY+STATUS+IN+THE+PAST+12+MONTHS+BY+SEX+BY+AGE&g=0100000US_0500000US18127_1400000US18127051101&tid=ACSDT5Y2021.B17001	
TABLE NOTES		
	Although the American Community Survey (ACS) produces population, demographic and housing unit estimates, it is the Census Bureau's Population Estimates Program that produces and disseminates the official estimates of the population for the nation, states, counties, cities, and towns and estimates of housing units for states and counties.	
	Supporting documentation on code lists, subject definitions, data accuracy, and statistical testing can be found on the American Community Survey website in the Technical Documentation section.	
	Sample size and data quality measures (including coverage rates, allocation rates, and response rates) can be found on the American Community Survey website in the Methodology section.	
	Source: U.S. Census Bureau, 2017-2021 American Community Survey 5-Year Estimates	
	Data are based on a sample and are subject to sampling variability. The degree of uncertainty for an estimate arising from sampling variability is represented through the use of a margin of error. The value shown here is the 90 percent margin of error. The margin of error can be interpreted roughly as providing a 90 percent probability that the interval defined by the estimate minus the margin of error and the estimate plus the margin of error (the lower and upper confidence bounds) contains the true value. In addition to sampling variability, the ACS estimates are subject to nonsampling error (for a discussion of nonsampling variability, see ACS Technical Documentation). The effect of nonsampling error is not represented in these tables.	
	The 2017-2021 American Community Survey (ACS) data generally reflect the March 2020 Office of Management and Budget (OMB) delineations of metropolitan and micropolitan statistical areas. In certain instances, the names, codes, and boundaries of the principal cities shown in ACS tables may differ from the OMB delineation lists due to differences in the effective dates of the geographic entities.	

	<p>Estimates of urban and rural populations, housing units, and characteristics reflect boundaries of urban areas defined based on Census 2010 data. As a result, data for urban and rural areas from the ACS do not necessarily reflect the results of ongoing urbanization.</p>
	<p>Explanation of Symbols:- The estimate could not be computed because there were an insufficient number of sample observations. For a ratio of medians estimate, one or both of the median estimates falls in the lowest interval or highest interval of an open-ended distribution. For a 5-year median estimate, the margin of error associated with a median was larger than the median itself.N The estimate or margin of error cannot be displayed because there were an insufficient number of sample cases in the selected geographic area. (X) The estimate or margin of error is not applicable or not available.median- The median falls in the lowest interval of an open-ended distribution (for example "2,500-")median+ The median falls in the highest interval of an open-ended distribution (for example "250,000+").** The margin of error could not be computed because there were an insufficient number of sample observations.*** The margin of error could not be computed because the median falls in the lowest interval or highest interval of an open-ended distribution.***** A margin of error is not appropriate because the corresponding estimate is controlled to an independent population or housing estimate. Effectively, the corresponding estimate has no sampling error and the margin of error may be treated as zero.</p>
COLUMN NOTES	None

Table: ACSDT5Y2021.B17001

Label	Porter County, Indiana		Census Tract 511.01, Porter County, Indiana	
	Estimate	Margin of Error	Estimate	Margin of Error
Total:	168,354	±496	6,085	±38
Income in the past 12 months below poverty level:	17,017	±1,804	379	±224
Male:	7,299	±922	161	±139
Under 5 years	655	±231	23	±36
5 years	82	±63	0	±17
6 to 11 years	578	±180	43	±65
12 to 14 years	720	±255	95	±69
15 years	190	±157	0	±17
16 and 17 years	219	±122	0	±17
18 to 24 years	1,121	±387	0	±17
25 to 34 years	900	±246	0	±17
35 to 44 years	845	±312	0	±17
45 to 54 years	722	±260	0	±17
55 to 64 years	827	±208	0	±17
65 to 74 years	262	±97	0	±17
75 years and over	178	±82	0	±17
Female:	9,718	±1,184	218	±124
Under 5 years	708	±198	0	±17
5 years	185	±130	0	±17
6 to 11 years	773	±235	83	±64
12 to 14 years	371	±170	0	±17
15 years	185	±115	0	±17
16 and 17 years	189	±125	0	±17
18 to 24 years	1,345	±299	0	±17
25 to 34 years	1,425	±298	0	±17
35 to 44 years	1,106	±193	108	±73
45 to 54 years	895	±288	14	±22
55 to 64 years	1,370	±312	0	±17
65 to 74 years	729	±337	0	±17
75 years and over	437	±128	13	±21
Income in the past 12 months at or above poverty level:	151,337	±1,942	5,706	±225
Male:	76,362	±982	2,932	±196
Under 5 years	3,797	±281	101	±61
5 years	828	±221	0	±17

Table: ACSDT5Y2021.B17001

	Porter County, Indiana		Census Tract 511.01, Porter County, Indiana	
Label	Estimate	Margin of Error	Estimate	Margin of Error
6 to 11 years	5,444	±493	109	±82
12 to 14 years	3,322	±407	114	±55
15 years	925	±169	87	±86
16 and 17 years	2,424	±233	200	±116
18 to 24 years	5,974	±403	319	±119
25 to 34 years	9,422	±290	357	±117
35 to 44 years	10,654	±317	351	±126
45 to 54 years	10,399	±284	420	±144
55 to 64 years	10,923	±257	495	±141
65 to 74 years	8,236	±138	227	±83
75 years and over	4,014	±179	152	±76
Female:	74,975	±1,286	2,774	±253
Under 5 years	3,637	±221	134	±88
5 years	921	±213	62	±57
6 to 11 years	5,592	±395	246	±126
12 to 14 years	2,541	±281	90	±64
15 years	1,093	±203	60	±70
16 and 17 years	2,022	±212	43	±43
18 to 24 years	5,024	±383	163	±97
25 to 34 years	8,843	±299	268	±128
35 to 44 years	10,347	±221	356	±94
45 to 54 years	10,309	±291	422	±170
55 to 64 years	11,015	±322	530	±153
65 to 74 years	8,256	±368	121	±69
75 years and over	5,375	±180	279	±115